

SPECIFIC PLAN

West Fairview Road

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SPECIFIC PLAN COMPONENTS

A range of housing types for all income levels, including:

- *Standard R-1 Single-Family*
- *Large Lot Single-Family*
- *Zero Lot Line Single-Family*
- *Small Lot Single-Family*
- *Senior Housing*
- *Multiple-Family Housing*
- *7% Moderate Income Housing Units*
- *3% Low Income Housing Units*

A recreational/open space park system which provides:

- *A Linear Parkway*
- *Neighborhood Park*
- *Bicycle/Pedestrian Pathway System*

SPECIFIC PLAN INFORMATION

Location: *Airline Highway/Fairview Road*

San Benito County General Plan Land Use Designation: *Area of Special Study*

City of Hollister General Plan Land Use Designation: *Residential Single-Family*

Urban Development Policy: *Within City Sphere of Influence*

Surrounding Land Uses:

North, South: Residential

West: Residential/Limited Agriculture

East: Grazing/Limited Agriculture

Sewage Treatment Service: *City of Hollister*

Water Service: *Sunnyslope Water District*

Police and Fire Protection: *City of Hollister*

Applicable School Districts:

Hollister Elementary School District

San Benito Joint Union High School District

SUMMARY

WEST FAIRVIEW ROAD SPECIFIC PLAN

A specific plan is used as a tool to implement the general plan. As its name implies, a specific plan is a detailed plan for the development of a specific area. It implements the local general plan by creating a bridge between general plan policies and individual development proposals. Ideally, a specific plan directs all facets of future development: from the distribution of land uses to the location and sizing of supporting infrastructure, from methods of financing public improvements to standards of development.

In the case of the West Fairview Road Specific Plan, the purpose of the specific plan effort is to provide comprehensive land use, circulation and infrastructure plans, development and design guidelines, housing strategies and implementation programs for guiding and ensuring the orderly development of the West Fairview Road area.

STATUTORY REQUIREMENTS

The West Fairview Road Specific Plan was prepared under the statutory requirements and guidelines defined in the current Office of Planning and Research publication "Specific Plans in the Golden State" (March, 1989). Section 65451 mandates that a specific plan contain:

- A. Text and diagram(s) which specify all of the following in detail:
 - 1) The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan.
 - 2) The proposed distribution, location, extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan.

- 3) Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable.
- 4) A program of implementation measures including regulations, programs, public works projects, and financing measures necessary to carry out paragraphs (1), (2), and (3).

B. A statement of the relationship of the specific plan with the general plan(s).

Prior to January 1, 1985 a specific plan was expected to address "each element of the general plan" as well as contain specific information related to land use and conservation patterns and regulations. AB 2038 (Stats. 1984, Ch. 1009) deleted those requirements, generalized the required contents of the specific plan, and standardized the administration and adoption of specific plans.

SPECIFIC PLAN ORGANIZATION

The West Fairview Road Specific Plan is organized into eight sections. Section 1.0, Introduction, provides a description of the project site and an overview of the project objectives. The purpose of the Specific Plan and the project's relationship to the City of Hollister General Plan is also discussed.

Section 2.0, Land Use, contains goals, objectives, policies and implementation actions which establish the intent of the land uses in the Specific Plan area, and the functional, locational and qualitative requirements of each.

Section 3.0, Housing, proposes an Affordable Housing Program that provides a range of housing opportunities in the Specific Plan area.

Section 4.0, Circulation, contains goals, objectives, policies and implementation actions associated with vehicle, bicycle, and pedestrian circulation facilities in the Specific Plan area.

Section 5.0, Resource Conservation, addresses the demands placed by future Specific Plan area residents on community and regional resources including potable water, air and water quality, and energy.

Section 6.0, Community Design, augments the policy statements presented in Sections 2.0 through 5.0 with more detailed design policies for architecture, landscaping, entrances, major intersections, signage, sound attenuation and lighting. Development standards, many of which are included in Appendix A, are referenced in Section 6.0. Combined, the policy statements, implementation measures and additional design standards are intended to serve as a framework

guiding day-to-day decisions associated with development in the West Fairview Road area.

Section 7.0, Public Services and Utilities, contains goals, objectives, policies and implementation actions associated with infrastructure development in the Specific Plan area. Included in this section are diagrams and policy statements addressing water, sanitary sewer and storm drainage. In addition, police and fire protection, schools, solid waste disposal and utilities are addressed.

Section 8.0, Plan Implementation, provides a capital improvements program and summarizes the means by which planned improvements may be financed. The relationship of the project to the California Environmental Quality Act is also discussed. A more detailed discussion of implementation is contained in a separate document entitled "Implementation Program." This document is intended to provide project proponents and City Staff with a guide to faithfully execute this Specific Plan.

Sections 1.0 through 7.0 contain a number of maps and diagrams. The maps and diagrams illustrate detailed information pertaining to the physical development of the West Fairview Road area, including acceptable land use locations and design specifications. Both the maps and the diagrams represent the legal authority vested in the plan to regulate land use and development.

SECTION 1.0

INTRODUCTION

1.1 BACKGROUND

In February 1990, the San Benito County Board of Supervisors and the City of Hollister City Council directed the preparation of a Specific Plan for the West Fairview Road area. Since the majority of the Specific Plan area is in the County of San Benito, the County initially assumed the role of Lead Agency and initiated the planning and preparation of the West Fairview Road Specific Plan. The function of the City of Hollister during the Specific Plan preparation process was originally as Responsible Agency.

In Fall 1992, approximately one-third of the Specific Plan area was annexed to the City of Hollister. Given the planned eventual annexation of the remainder of the Specific Plan area, the City and County subsequently switched roles, whereby the City became the Lead Agency for the project and EIR, and the County became a Responsible Agency. The City and the County continue to work cooperatively on the project.

The intent of the Specific Plan is to address the apparent need for coordinating land use, public infrastructure and servicing needs for the area. This need was identified in Environmental Impact Reports prepared earlier for developments proposed on properties within the project area. A request to authorize the preparation of a Specific Plan for the West Fairview Road Property Owners Group was taken to the County Board of Supervisors and the Hollister City Council for independent actions. During that time, the following general parameters guiding the preparation of the Specific Plan were defined:

- Since the entire Plan area will eventually be annexed to the City, the City will provide outside sewer services to the project area.
- The project will not be subject to the City of Hollister's Residential Development Ordinance #767.
- The City and County will expedite the approval and planning process.

Since the initiation of Specific Plan preparation, the County, the City and the affected property owners have worked together to establish a land use plan that serves the objectives set forth in both the County and City General Plans and meets the needs of the community and individual objectives of the property owners.

A Joint Planning Commission Subcommittee was formed to evaluate the land use plan and circulation and policy components of the Specific Plan as they were developed. The Subcommittee was comprised of four Planning Commissioners, two each from the City and County.

Numerous professional consultants further contributed to the preparation of this plan, including planning, engineering, traffic, economic and environmental consultants.

Numerous public hearings were held by the City of Hollister Planning Commission. At these meetings, city staff, Commissioner and private citizen comments further refined and shaped the plan.

A detailed description of the proposed land uses and supporting infrastructure, prepared in accordance with the State of California Office of Planning and Research (OPR) requirements for the preparation of specific plans, is contained herein.

1.2 SITE LOCATION AND DESCRIPTION

The majority of the West Fairview Road Specific Plan area is located in San Benito County, within the Sphere of Influence of the City of Hollister. Figure 1.1 presents the regional location of the subject area, while Figure 1.2 illustrates the vicinity of the Specific Plan area. As indicated in Figure 1.2, the project area is bordered by Fairview Road on the east and Airline Highway on the south.

The existing Specific Plan area is illustrated in Figure 1.3. The project area is generally vacant of structures, with the exception of a few single family residences with frontages along Fairview Road. Since the area has historically been used for limited agricultural and grazing activities, farming related structures, such as barns and sheds, are located throughout the area. A small walnut orchard is also located in the Project area.

For planning purposes, the West Fairview Road area has been divided into two planning areas as noted on Figure 1.4. The acreage and Assessor Parcel Numbers (APN) for all parcels included within these two planning areas are also summarized on Figure 1.4.

Planning Area A is comprised of nine parcels, totaling approximately 296 acres. Parcels in Area A range in size from 2.0 to 100.0 acres. The owners of all Planning Area A parcels have actively participated in the preparation of the Specific Plan.

The largest of the properties (Parcel A9 in Figure 1.4) is the subject of an existing development agreement between the City of Hollister and the owner. That development agreement specifies allocation of a total of 629 units to be constructed on Parcel A9 (459 units) and a 35-acre parcel (170 units) to the west known as "Crestview Estates." Construction of these units is to be phased over a nine-year period. The timing and financing of on- and off-site infrastructure

FIGURE 1.1

Regional Location

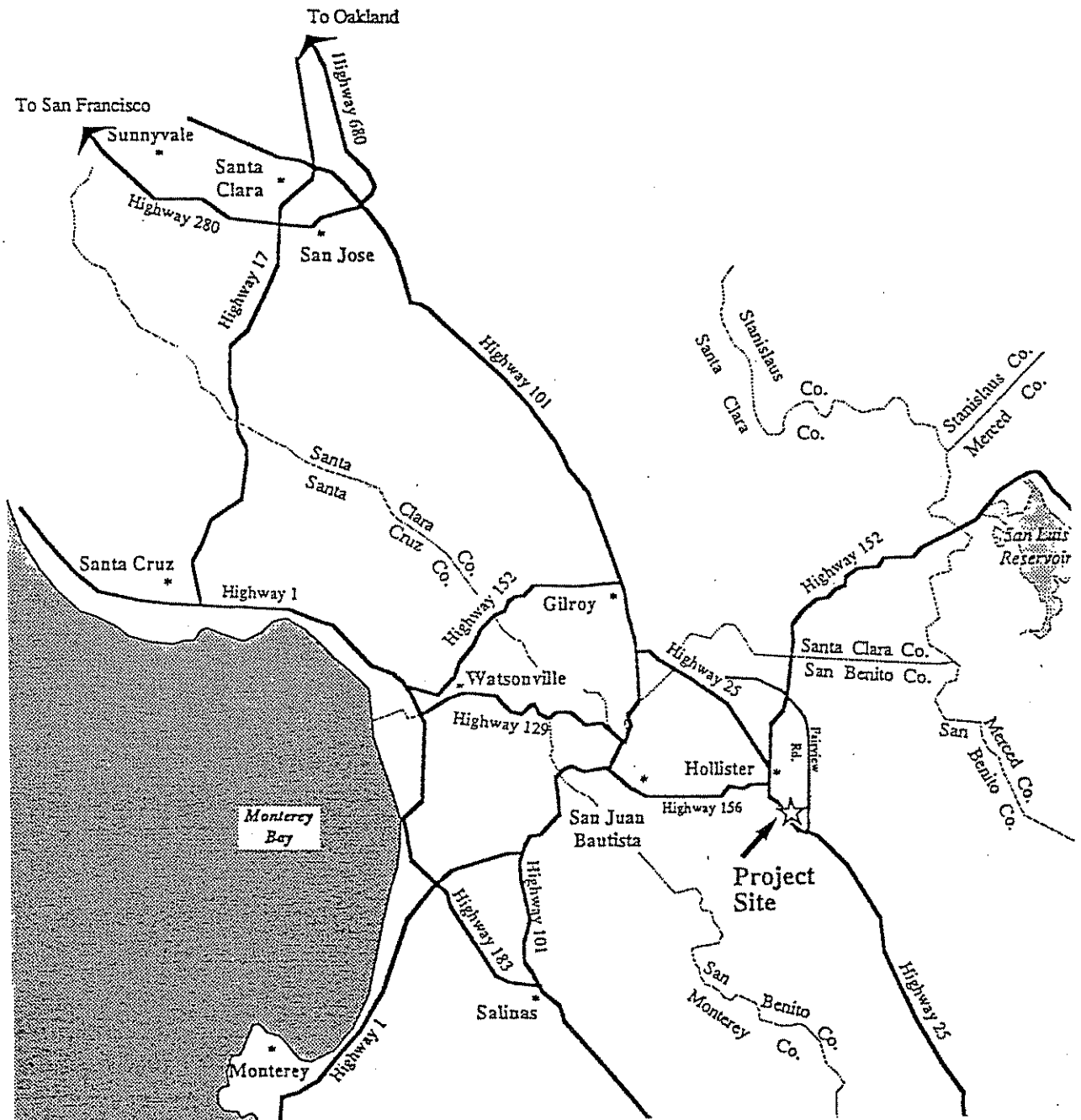


FIGURE 1.2

Project Vicinity

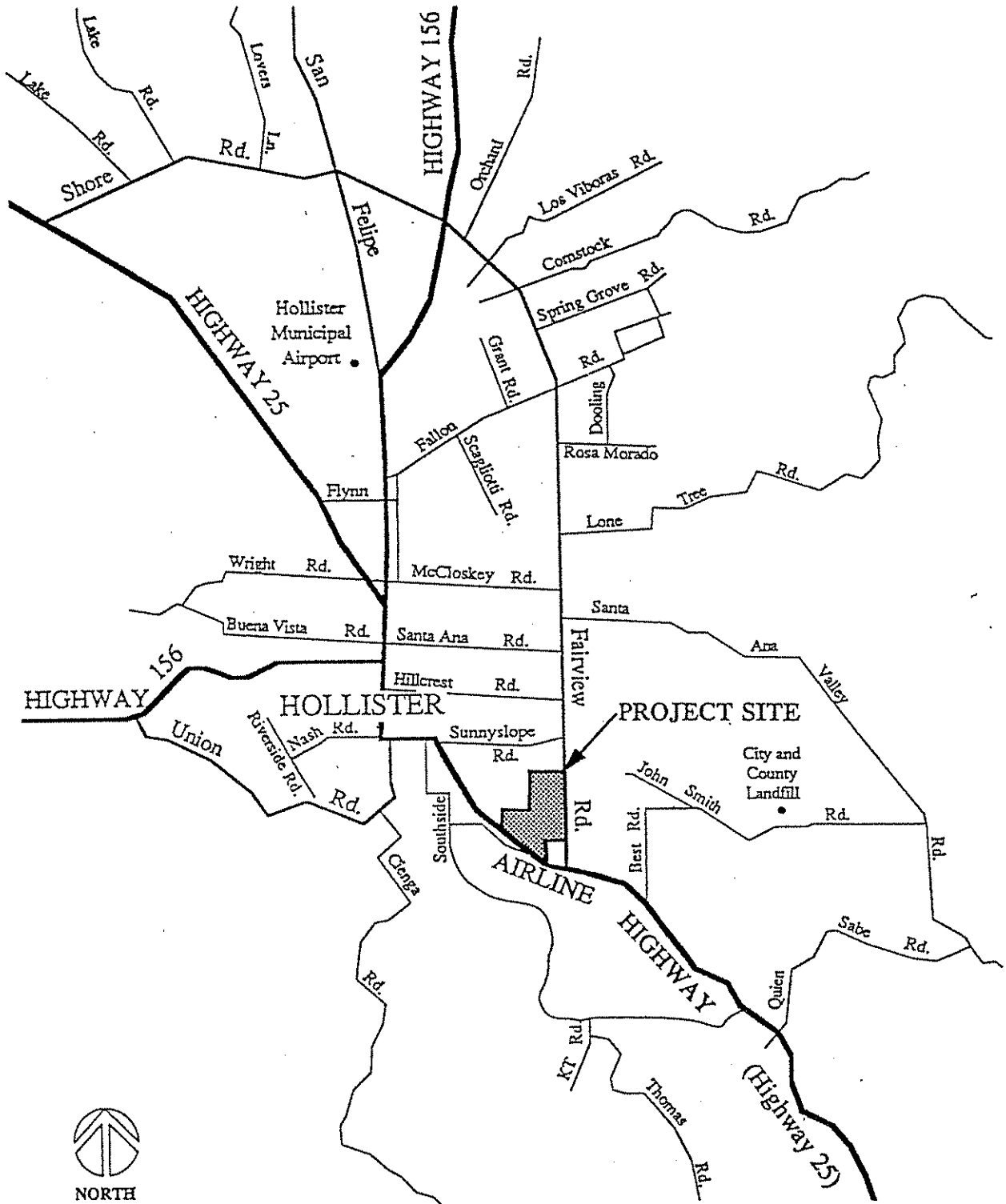


FIGURE 1.3

Existing Specific Plan Area

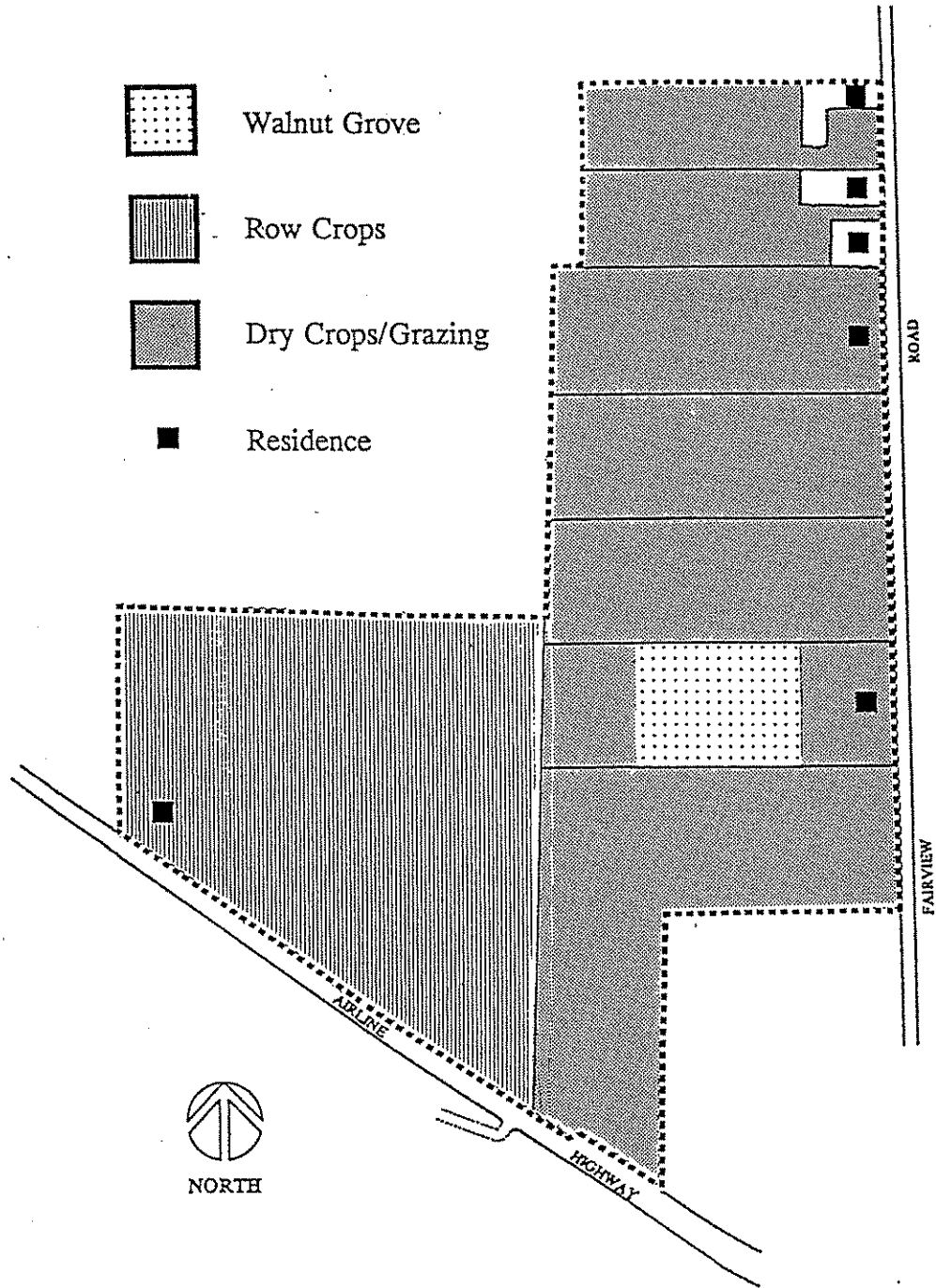


FIGURE 1.4

Specific Plan Planning Areas

EXISTING PARCEL SIZES AND APNs

	<u>APN</u>	<u>ACRES</u>
<u>Planning Area A</u>		
A1	20-27-40	1.98
A2	20-27-39	14.82
A3	20-27-41	15.70
A4	20-31-05	27.34
A5	20-31-06	27.66
A6	20-31-07	27.07
A7	20-31-08	28.12
A8	20-31-09	53.29
A9	20-30-02	100.00

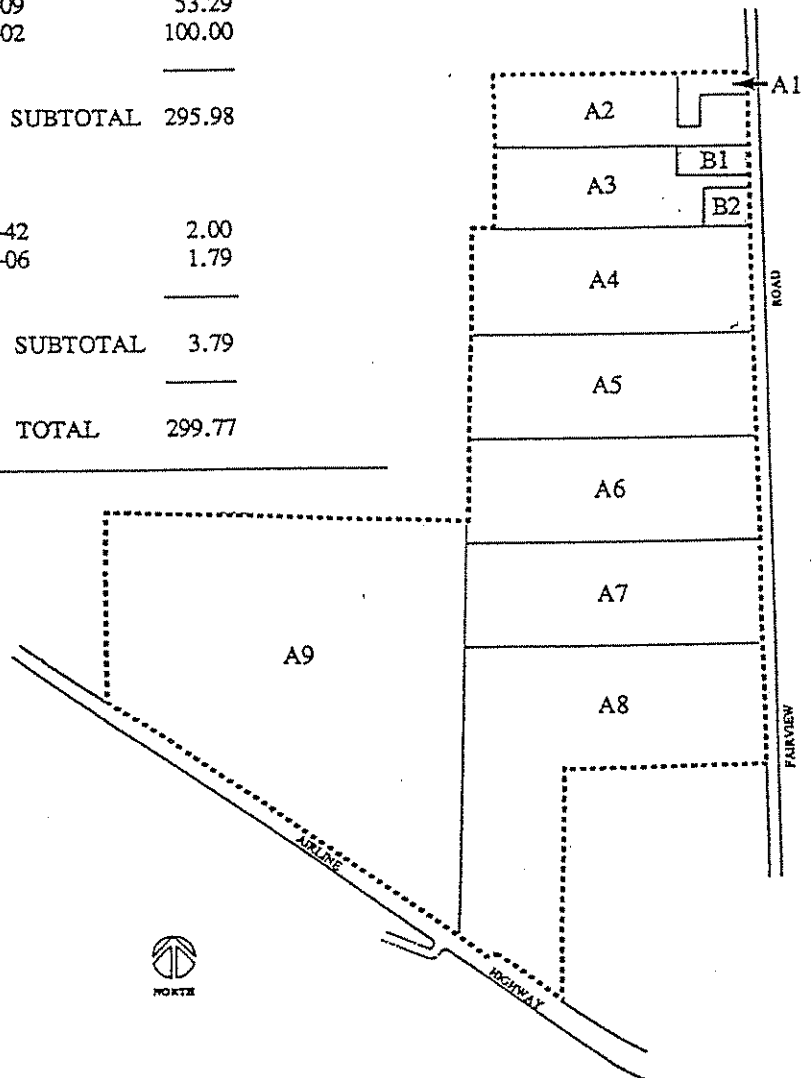
SUBTOTAL 295.98

Planning Area B

B1	20-27-42	2.00
B2	20-27-06	1.79

SUBTOTAL 3.79

TOTAL 299.77



improvements is also specified in that development agreement. Parcel A9 has its own Tentative Map and environmental impact report and has been annexed to the City of Hollister, a Tentative Map has been approved and an EIR has been certified.

Parcel A9 (known as Valley View Estates) is consistent with the overall general land use/density requirements of the Specific Plan. However, since this parcel has already received Tentative Map approval, EIR certification, and Development Agreement adoption, it will be bound only to those goals, objectives, policies, standards, and mitigation measures which are consistent with requirements identified in said Tentative Map, EIR and Development Agreement. If there is a conflict between a requirement within the West Fairview Road Specific Plan or EIR and one within the Valley View Estates Tentative Map, EIR or Development Agreement, then the Valley View Estates documents shall supersede. In addition, should the Valley View Estates Tentative Map lapse, or the Development Agreement be terminated, then the conditions cited within the West Fairview Road Specific Plan and EIR shall apply to the remaining undeveloped areas of the Valley View Estates property.

Planning Area B is comprised of two smaller parcels with frontage on Fairview Road. Currently one single family residence is located on each parcel. At the request of the County and City these parcels have been included in the Specific Plan to ensure that all future land use decisions on these parcels are consistent with the design criteria established by the Specific Plan. In addition, since direct access from these parcels to Fairview Road will be prohibited with the build-out of the Specific Plan area, by including the parcels in the Specific Plan, future internal access can be guaranteed.

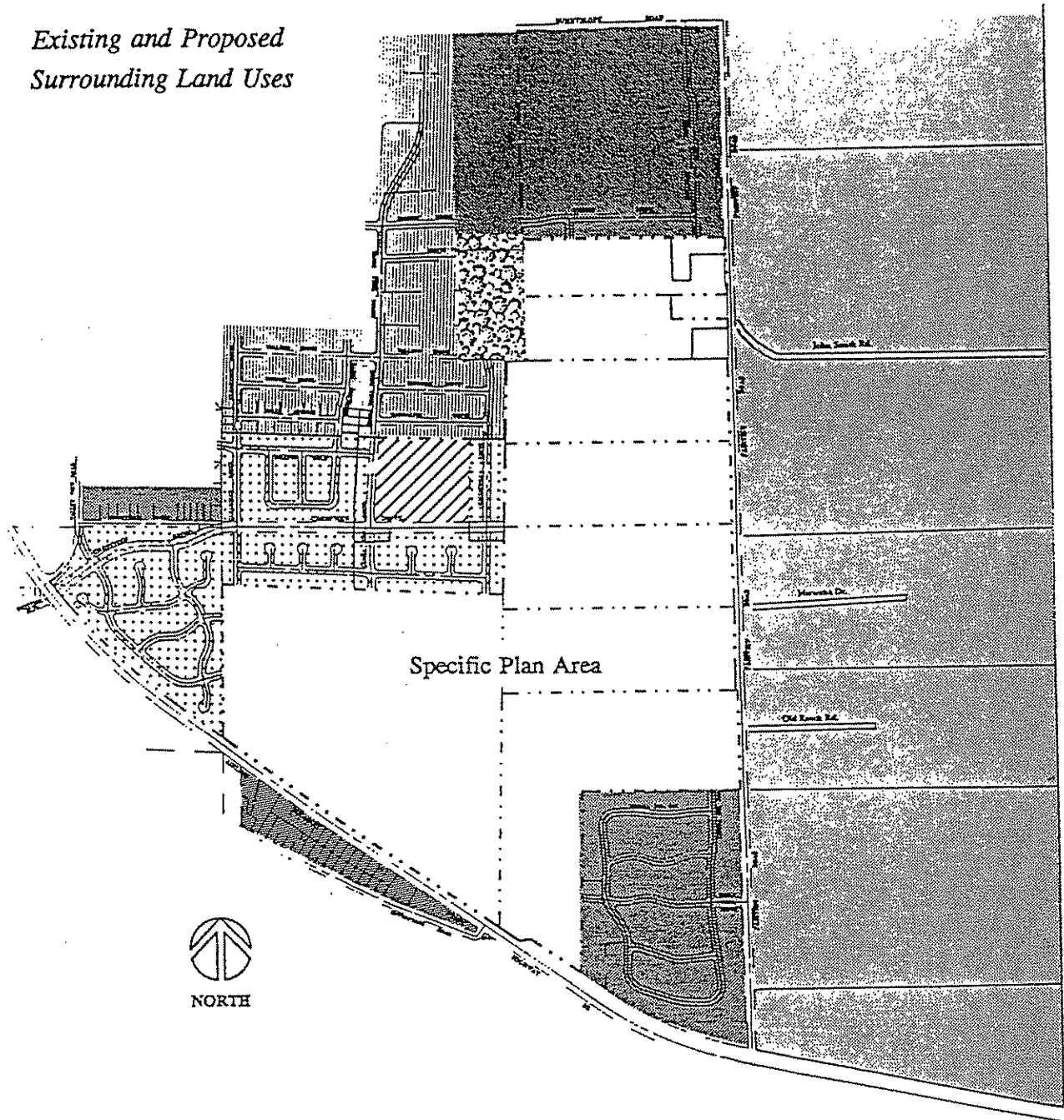
1.3 SURROUNDING LAND USES





The West Fairview Road Specific Plan area is surrounded primarily by existing and proposed residential uses. In addition, existing public/quasi-public and proposed commercial and public/quasi-public uses are located within the immediate vicinity of the project area. Figure 1.5 illustrates these surrounding land uses.



As indicated on Figure 1.5, existing residential uses are located to the north and south of the Specific Plan area. Holliday Estates, a 47-unit County subdivision of one-acre lots is located to the north of the project area. Walnut Park #7 (A & B) a standard, single family City subdivision (6,000 - 7,000 sq. ft. lots) is located to the north and west. An existing orchard is also located adjacent to the northwest corner of the Plan area. Cielo Vista Estates, a 75-unit County subdivision of half-acre lots, is located to the south at the intersection of Airline Highway and Fairview Road. Finally, a single family County subdivision on minimum one-acre lots is located to the southwest across Airline Highway on Enterprise Road.

FIGURE 1.5

Existing and Proposed Surrounding Land Uses



- | CITY | |
|---|---|
|  | Existing Residential |
|  | Existing Public/Quasi-Public
(Cerra Vista School, Hollister
Elementary School District) |
|  | Proposed Residential
(Existing Use: Vacant) |
|  | Existing Walnut Grove |

- | COUNTY | |
|--|--|
|  | Existing Residential |
|  | Proposed Residential, Commercial,
& Public/Quasi-Public; East
Fairview Road Specific Plan
(Existing Use: Dryland Agriculture) |

Proposed residential uses include: the Walnut Park Subdivision #13 located to the north of the Specific Plan area, the Crestview Estates Subdivision to the west and the East Fairview Specific Plan residences to the east. The proposed Walnut Park and Crestview Estates Subdivisions are located within the City, while the East Fairview Specific Plan area is in the County. The East Fairview Specific Plan proposes 2,400 residential units and commercial, public/quasi-public and open space/park uses. Currently, the East Fairview Road area consists of nut orchards, dryland grass cultivation, and five single family homes, including a few barns and accessory structures. The existing public/quasi-public use within the immediate vicinity of the West Fairview Road Specific Plan area is the Cerra Vista Elementary School to the west. As noted, proposed public/quasi-public and commercial uses in the project vicinity include an elementary school and neighborhood shopping facilities planned as components of the East Fairview Road Specific Plan.

1.4 PLANNING BOUNDARIES

As illustrated on Figure 1.6, the majority of the Specific Plan area is presently outside of the City of Hollister, under the jurisdiction of San Benito County. In addition, the project area is within the Sphere of Influence of the City. Figure 1.6 illustrates the boundaries of the City Urban Service Area which is generally adjacent to the northern and western boundaries of the Plan area. The City Limit and Urban Service Area boundaries encompass Parcel A9. The remaining portion of the western boundary to the Specific Plan area is contiguous to the existing City limits.

1.5 EXISTING GENERAL PLAN DESIGNATIONS

In the course of preparing an updated Land Use Element of their General Plan (adopted July 14, 1992), the County of San Benito redesignated that portion of the Specific Plan area remaining within the County "Area of Special Study." This designation applies to areas of the County where a combination of the following criteria apply: 1) More concentrated development than is presently allowed may be desirable provided a comprehensive plan for public services and resource conservation is integrated in order of priority into a specific plan, community plan, or area plan; 2) As a result of prior, piecemeal subdivisions, a plan for integrated development and coordination of governmental services and/or community facilities is desirable; 3) The County has identified the area as a potential area for commercial, industrial and/or residential development.

The existing City General Plan land use designation for the area is "Residential Single Family," requiring minimum 6,000 square foot lots (5.0 to 6.0' dwelling units per gross acre).

¹Personal communication with Bill Card, March 9, 1993.

FIGURE 1.6

*City of Hollister Sphere of Influence,
Urban Service Area, and City Boundaries*

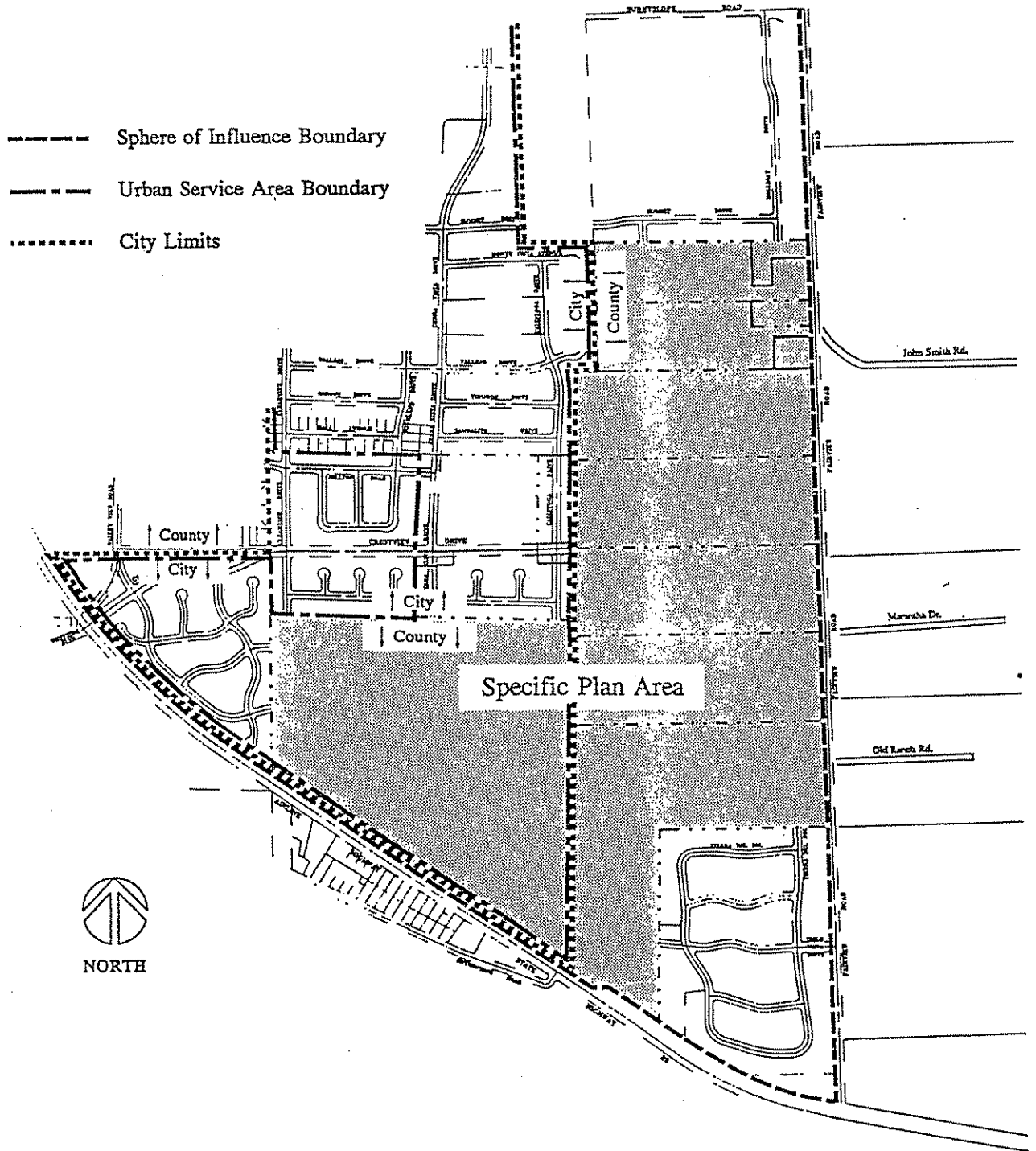
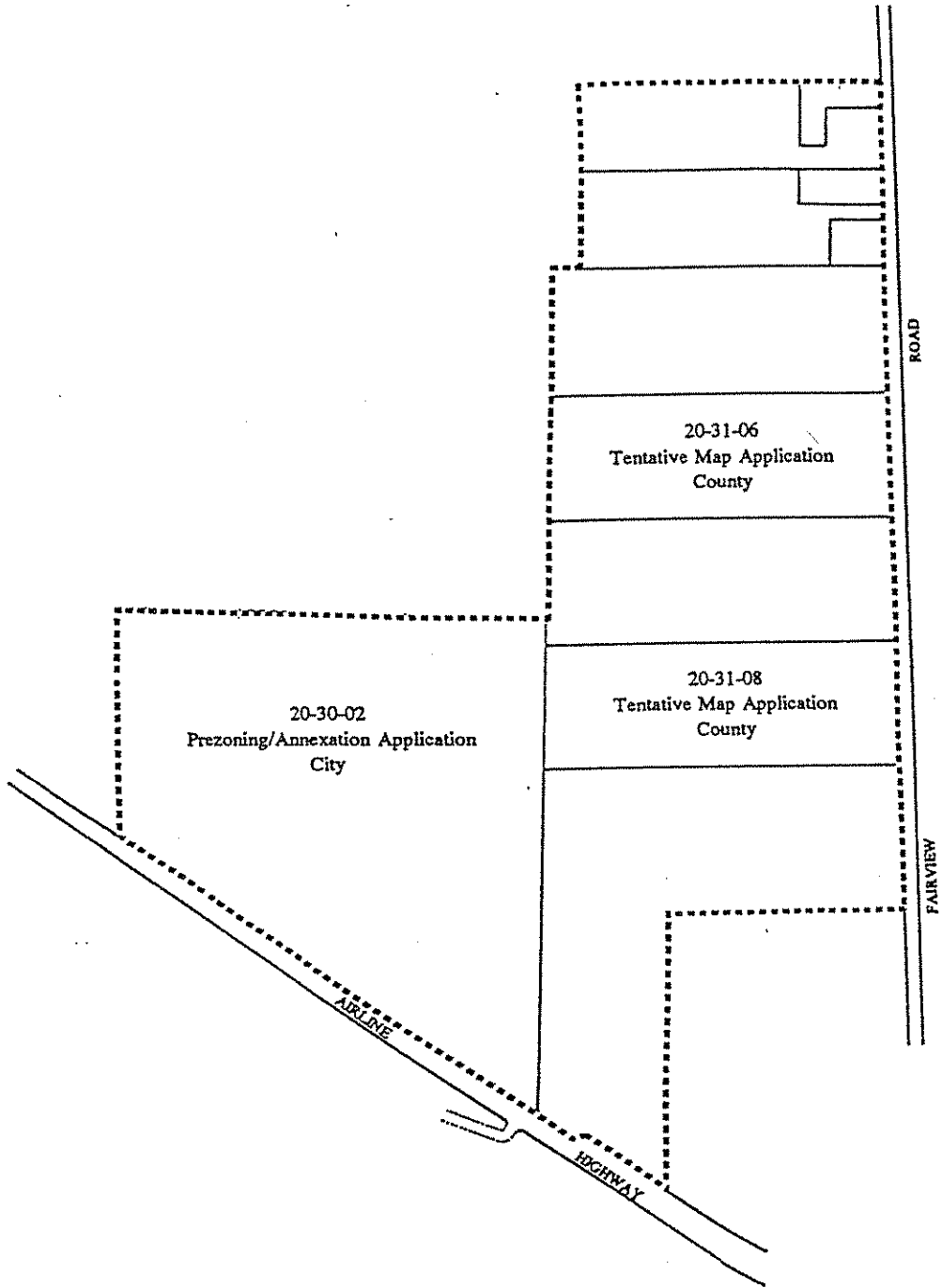


FIGURE 1.7

Previous Development Proposals



The City is currently working on a major revision to their General Plan. The Hearing Draft General Plan (prepared in August 1992) indicates a preliminary land use designation of Low Density Residential (LDR) for the West Fairview Specific Plan area. The LDR classification is generally characterized for single family uses including: single family detached residential units, zero lot line single family residential, or residential planned development. A density of 1 to 8 units per net acre or 0.75 to 6 units per gross acre is permitted. Medium Density Residential (MDR) uses, such as zero lot line single family residential, duplex, triplex and fourplex may be included in LDR developments, subject to approval by the Planning Commission of a Conditional Use Permit. It is anticipated that the revised General Plan will be adopted by the City Council in Spring, 1994.

1.6 PURPOSE OF THE SPECIFIC PLAN

The overriding purpose of the Specific Plan is to provide comprehensive land use, circulation and infrastructure plans, development and design policies, housing strategies and implementation programs for guiding and ensuring the orderly development of the West Fairview Specific Plan area. As illustrated in Figure 1.4, the West Fairview Specific Plan area is comprised of eleven properties, where eight of these properties are larger than fourteen acres in size. The West Fairview Road Specific Plan area has experienced great development pressures over the past few years. Development proposals have been received by the County and City on many of these larger parcels. As illustrated on Figure 1.7, tentative map applications have been received by the County on parcels 20-31-06 and 20-31-08. In the City, a rezoning/annexation application has been received and recently approved for parcel 20-30-02.

The entire Specific Plan area is within the City Sphere of Influence. Annexation of the remainder of the Plan area to the City is planned immediately following adoption of this Specific Plan. Specific Plan property must be annexed to the City prior to obtaining Tentative Map approval for proposed development. As described in this Specific Plan, services throughout the Plan area shall be coordinated and shall be designed to meet City standards.

The Specific Plan area is approximately 300 acres in size. As defined by the "City of Hollister Draft Traffic and Circulation Master Plan for the West Fairview Area," major circulation improvements are planned for the project vicinity. The Specific Plan coordinates these major improvements with the internal roadway networks required to service individual parcels. The Specific Plan Circulation Plan ensures a roadway network will be developed for the area that is continuous, efficient and safe.

Under California law a jurisdiction may use a specific plan to develop specific regulations, programs and legislation to implement that jurisdiction's adopted General Plan. As its name implies, a specific plan is a detailed plan for the development of a specific area. The law requires that a specific plan include text and diagrams addressing:

- the distribution, location, and intensity of land uses, including recreational/open space, within the plan area;
- the distribution, location and capacity of major infrastructure improvements, including transportation, sewage, storm water drainage, solid waste disposal and energy systems;
- standards and criteria for development and utilization of natural resources; and
- implementation measures, including capital improvements and financing mechanisms, necessary to execute the Plan.

In essence, a specific plan offers the opportunity to translate the community's broad policy goals and objectives set forth in their General Plans into a mechanism for guiding actual development.

Through the development of a Specific Plan for the West Fairview Road area, comprehensive land use, circulation and infrastructure plans may be developed and implemented, ensuring orderly development of the West Fairview Specific Plan area.

1.7 RELATIONSHIP BETWEEN THE GENERAL PLAN AND SPECIFIC PLAN

Since the West Fairview Road Specific Plan reflects a cooperative planning effort on the part of the City and the County, project goals, objectives, policies and implementation actions set forth in the Plan are consistent with, and will help to implement, both the County General Plan and the revised City General Plan.

General Plan policies cover a range of topics including natural resources, environmental constraints, human resources, community development and transportation planning. The West Fairview Road Specific Plan incorporates elements of both the County and City General Plans, zoning ordinances, and other local and regional plans as goals, objectives, policies, and implementation actions for the orderly development of the West Fairview Road area. All individual development proposals (i.e., subsequent tentative map applications, rezonings, etc.) within the Specific Plan area will be subject to the policy statements set forth in this Specific Plan.

The entire Specific Plan area is to be proposed for annexation to the City immediately upon adoption of the Specific Plan. By State law, the Specific Plan must be consistent with the goals, objectives and policies contained in the City General Plan. An analysis describing the relationship between Specific Plan policies and the City General Plan is contained in the West Fairview Specific Plan EIR.

The existing City General Plan land use designation for the area is "Residential Single Family," requiring minimum 6,000 square foot lots (5.0 to 6.0 dwelling units per gross acre). Therefore, the project's proposed density of 5.4 dwelling units per gross acre is consistent with the existing City General Plan Land Use.

The City of Hollister is in the process of updating the City General Plan. The purpose of the updated General Plan is to provide a comprehensive land use plan to replace the Hollister General Plan which was adopted in 1976.

To date, five reports have been prepared as a part of the General Plan Program. These reports include:

1. Work Program, Schedule of Activities, and Guidelines;
2. Issues and Policy Choices;
3. Alternative Scenarios;
4. Preliminary Draft General Plan; and
5. Hearing Draft General Plan.

A land use designation of Low Density Residential (LDR) has been proposed for the West Fairview Specific Plan area. The LDR classification is generally characterized for single family uses including: single family detached residential units, zero lot line single family residential, or residential planned development. A density of 1 to 8 units per net acre or .75 to 6 units per gross acre is permitted. Medium Density Residential (MDR) uses, such as zero lot line single family residential, duplex, triplex and fourplex may be included in LDR developments, subject to approval by the Planning Commission of a Conditional Use Permit.

The West Fairview Specific Plan land use and density has been developed to be consistent with the City's revised Hearing Draft General Plan. The project's proposed average density of 5.4 units per gross acre is consistent with the proposed Low Density Residential designation.

1.8 PROJECT OVERVIEW

The West Fairview Road Land Use Plan, described in detail in Section 2.2 and illustrated in Figure 2.1, summarizes the future residential and recreational/open space uses in the Specific Plan area. Table 1.1 summarizes the proposed land uses and their acreage. The land uses set forth in the Specific Plan are as follows:

Residential

Development of the West Fairview Road Specific Plan area may consist of a variety of residential units including large lots, standard R-1 lots, small lot single family, zero lot line, duettes, multiple family and senior housing. Lot sizes will range from a minimum of 20,000 square feet for large lots, to a minimum of 3,500 square feet per duette unit. Section 6.3, Development Standards, defines the lot size, setbacks, site coverage, height limitations and parking requirements for each housing type. An overall maximum density of 5.4 dwelling units per gross acre is proposed.

Recreational/Open Space

A linear park system, including bicycle and pedestrian trails, with interspersed larger parkland areas, is a critical component in the design of the West Fairview Road Land Use Plan. An on-site neighborhood park will terminate the linear park. The park system will provide recreational and aesthetic opportunities to all project residents. In addition, as discussed in Sections 5.2 and 7.3, Hydrology and Storm Drainage, the linear and neighborhood park system would also serve as a storm drainage channel and detention system during peak storm periods.

TABLE 1.1 - LAND USES AND ACREAGE

<u>LAND USE</u>	<u>ACRES</u> (net)
Residential	207.2
• With Large Lot Overlay 1.0 - 2.0 DU/Ac. (14 acres)	
• With Medium Density Overlay 8.0 DU/Ac. (7.0 acres)	
Recreational/Open Space	11.1
Landscape/Noise Buffers	3.7
Roadways*	74.0
	<hr/> 296.0
* Assuming 25% gross acreage for roadway improvements.	

Landscape/Noise Buffers

To enhance the living environment for project residents, landscape/noise buffers are proposed along the project frontages with Airline Highway and Fairview Road. The landscaping and noise attenuation design policies defined in Section 6.0 will ensure that the buffers will not only provide noise attenuation but will also be visually appealing.

1.9 PLAN ADOPTION

This Specific Plan shall be adopted by resolution. If the Specific Plan is adopted prior to the City's adoption of the revised General Plan, changes to the City's existing "Single Family Residential" designation to "Single Family Residential/West Fairview Road Specific Plan" are proposed. If this Specific Plan is adopted following adoption of the City's revised General Plan, changes to the revised General Plan from "Low Density Residential" to "Low Density Residential/West Fairview Road Specific Plan" are proposed. Changes in Project area zoning will also follow the adoption of the Specific Plan. The Project area is to be zoned and rezoned Planned Development as appropriate.

1.10 SEVERABILITY CLAUSE

In the event that any policy, condition, program, or portion of this Specific Plan is held invalid or unconstitutional by a California or Federal Court of competent jurisdiction, such portions shall be deemed separate, distinct, and independent provisions, and the invalidity of such provisions shall not affect the validity of the remaining provision thereof.

SECTION 2.0

LAND USE

This Section contains goals, objectives, policies and implementation actions which establish the intent of land uses set forth in this Specific Plan, and describes the functional, locational, and qualitative requirements of each proposed land use. They are also intended to guide future individual development plans that will follow once the Specific Plan is adopted.

Section 6.0, Community Design, augments the policy statements presented in this Section with more detailed design policies. Examples include design policies for architecture, landscaping, entrances, major intersections, signage, sound attenuation and lighting. Combined, these policy statements are meant to serve as a framework to guide day-to-day decisions associated with future development of the West Fairview Road area.

2.1 LAND USE GOALS, OBJECTIVES AND POLICIES

Described below are the land use goals, objectives, policies and implementation actions for future development in the West Fairview Road area. The goals and objectives address the proposed residential and recreational/open space uses within the Specific Plan area. The policies expand on the goals and objectives. Implementation actions articulate specific actions or measures that effectuate policies.

All policy statements contained in this Specific Plan are based on, and are consistent with, goals and policies defined in the City of Hollister General Plan. The Specific Plan policies are also responsive to specific development opportunities and site constraints identified during the preparation of this Plan.

The following goals, objectives, policies and implementation actions expanded upon by a subsequent narrative description of proposed land uses and supporting illustrations.

Goal: Equal opportunities for all residents within the community to enjoy quality living standards.

Objective 1: Residential neighborhoods that are compatible with the existing character of the City of Hollister.

Policy 2.1.1: All neighborhoods in the project area should be located and designed to be mutually compatible and complement the existing character of the City of Hollister.

- Actions:
- a. All residential neighborhoods shall be designed and developed in accordance with the policies and actions set forth in Section 6.0, Community Design.
 - b. All individual residential projects shall be subject to the design review and approval of the City Planning Director to ensure conformance with this Plan.

Objective 2: A mix of housing types and densities.

Policy 2.2.1: A range of housing types and densities, which include single and multiple family homes, shall be allowed within residentially designated areas.

- Action:
- a. Residential uses within the Specific Plan area shall be developed in accordance with the Development Standards set forth in Section 6.3 of this Plan.

Policy 2.2.2: The range of housing types shall include smaller, more affordable units.

- Actions:
- a. Development Standards set forth in Section 6.3 of this Plan allow the development of small lot, zero lot line, duette and multiple family housing units.
 - b. Section 3.2, Affordable Housing Program, mandates that a percentage of all dwelling units constructed must be affordable to low and moderate income households.

Objective 3: Residential neighborhoods that are safe for residents, particularly children.

Policy 2.3.1: Roadways and pedestrian paths throughout the project area shall be designed to provide for safe and efficient movement.

- Action:
- a. Roadways and pedestrian paths shall be designed in accordance with the policies and actions set forth in Section 4.0, Circulation.

Objective 4: Neighborhood and area-wide recreational and open space amenities.

Policy 2.4.1: A linear and neighborhood park system shall be provided within the project area.

Action: a. As illustrated in Figure 2.1, Land Use Plan, approximately 7-8 acres of land bordering Crestview Drive and the main project arterial shall be designated for a linear parkway. The parkway will terminate to the south at an approximately 5 acre neighborhood park.

Policy 2.4.2: A bicycle/pedestrian pathway shall be incorporated into the linear and neighborhood park system.

Action: a. As illustrated in Figure 4.6, Bikeway System, and Figure 4.7, Bikeway Cross-sections, two-way bike paths shall be constructed within linear parkways.

Policy 2.4.3: Future, more detailed planning and design of the park system and bikeways shall be consistent with the guidelines set forth in the City of Hollister Parks and Recreation Master Plan and San Benito County Bikeway Plan. (Refer to Appendix A, Design Standards).

Action: a. The City Public Works Director, Planning Director and Parks and Recreation Director shall review designs for the neighborhood park and linear park to ensure consistency with the City of Hollister Parks and Recreation Master Plan and San Benito County Bikeway Plan.

Policy 2.4.4: Local streets within neighborhoods shall be designed to enhance the visibility of park system components.

Action: a. As illustrated in Figure 2.1, Land Use Plan, Park and recreation areas are located to provide for ease of access by pedestrians and bicyclists.

Policy 2.4.5 Neighborhood and linear park areas shall be designed and landscaped to facilitate surveillance by adjoining residents, security services and police.

Actions: a. Final designs for all park areas shall be consistent with the City of Hollister Parks and Recreation Master Plan and Streetscape Improvement Plans.

b. Final designs for the neighborhood park and linear park areas shall be subject to review and approval by the City Parks and Recreation Director.

Policy 2.4.6: The park system within the Specific Plan area shall be maintained by a Lighting and Landscaping Maintenance District.

- Action: a. The City of Hollister and project developers shall establish a Lighting and Landscaping Maintenance District, and shall enter into a Lighting and Landscaping Maintenance Agreement, prior to Tentative Map approval.

Objective 5: Neighborhoods that are quiet and buffered from noise and other nuisance factors.

Policy 2.5.1: Residences on lots adjacent to arterial streets shall be oriented with rear or side yards toward arterial streets wherever possible.

2.2 LAND USE PLAN PROPOSAL

Figure 2.1 illustrates the proposed land use plan for the West Fairview Road Specific Plan area. The land use goals, objectives, policies and implementation actions established in Section 2.1 serve as the conceptual basis for the land use plan. A discussion of the components of the land use plan and supporting illustrations are provided below.

Residential

A project-wide maximum density of 5.4 dwelling units per gross acre, or 1594 units total is proposed. Table 2.1 summarizes the number of dwelling units allowed per parcel based on these densities.

Policies set forth in this Section, and in Section 6.0, Community Design, encourage all residences to be mutually compatible, and compatible with the existing character of the City of Hollister. For example, West Fairview neighborhoods will be designed using a "modified grid" approach to street and lot layout. Street trees will line all roadways.

Plan policies also promote a range of housing opportunities to accommodate a range of income levels. The land use plan illustrates a base, project-wide density of 5.4 dwelling units per gross acre. The base density is modified by three overlay land use categories: Large Lot Residential (1.0-2.0 du/ac); Medium Density Residential (8.0 du/ac) and Parkland (linear parkway and

FIGURE 2.1

Land Use Plan

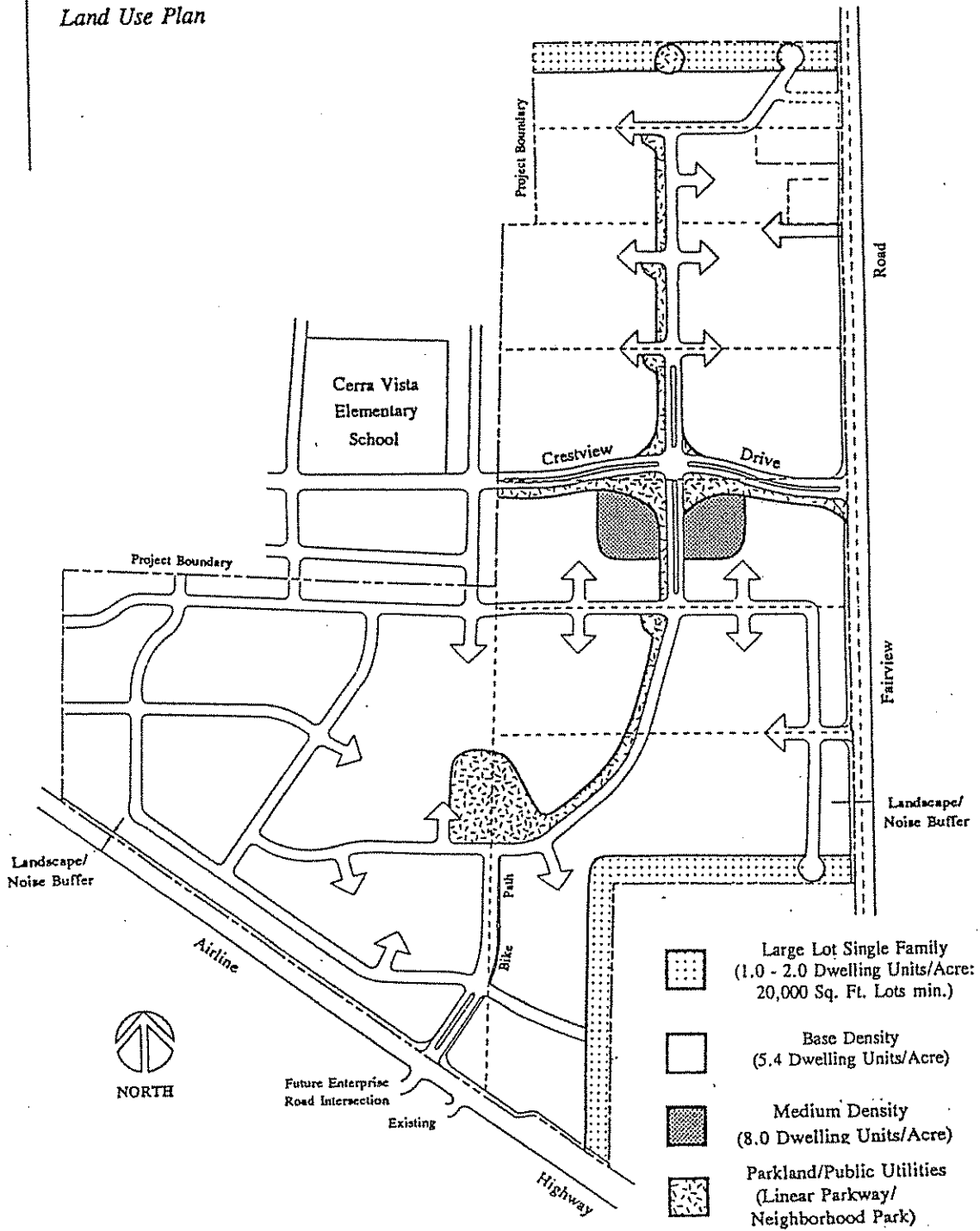


TABLE 2.1 - ALLOWABLE DWELLING UNITS PER PARCEL

<u>Parcel</u>	<u>Acres</u>	<u>Total Proposed Units</u> ¹
A1	1.98	10
A2	14.82	80
A3	15.70	84
A4	27.34	147
A5	27.66	149
A6	27.07	146
A7	28.12	151
A8	53.29	287
A9	<u>100.00</u>	<u>540</u> ²
TOTALS	295.98	1,594

¹ Based on 5.4 dwelling units per acre.

² Valley View Estates property will be subject to the West Fairview Road Specific Plan if its tentative map lapses.

neighborhood park). However, a project-wide density of 5.4 units per gross acre, or a total of 1594 units, may not be exceeded.

As illustrated in the Land Use Plan, Large lots ranging from 20,000 square feet minimum will provide transitional buffers to the half-acre and one-acre subdivisions located to the south and north of the Specific Plan area, respectively. Medium density residential development (8.0 dwelling units per acre) will be located at the intersection of the project arterial and Crestview Drive. Medium density residential units may include, small lot, zero lot line and multi-family dwellings.

The remainder of the West Fairview Specific Plan area will be developed at densities that yield a maximum project gross density of 5.4 dwelling units per acre. A mix of housing types is allowed to achieve this density. For example, an individual parcel may contain a combination of standard lots (6,000-8,000 sq. ft.), small lot single family and duette units. The mix of housing types on any Specific Plan parcel shall be at the discretion of the individual property owners, subject to City design review and approval. Such a housing type mix will provide a refreshing deviation from standard subdivisions and provide a range of housing types for all

income levels. Community design policies defined in Section 6.0 of this Plan ensure that future developments on all Specific Plan parcels will be mutually compatible.

Specific Plan policy also requires the development of affordable dwelling units. An affordable housing program has been developed and is included as Section 3.2 of this Plan. The purpose of the program is to provide units to a broad range of the community. Policies defined in Section 6.0, Community Design, will ensure that the architectural design and treatment of below market rate units will be visually compatible with market rate units.

Recreational/Open Space

Approximately eleven acres of the West Fairview area is designated for recreational/open space use. Policies set forth in the Specific Plan ensure the provision of recreational and open space amenities via the development of a linear parkway, bicycle/pedestrian path system and neighborhood park. As presented in Section 5.2, Specific Plan policies also encourage the protection of natural drainage and water recharge areas through coordinating the location of park/open space uses with such natural hydrological features where feasible.

Figures 2.2 and 2.3 illustrate the Specific Plan recreational/open space system. A linear park system, which runs parallel to the project north-south collector and Crestview Drive, serves as the back bone of the West Fairview park system. Two-way bike and pedestrian paths are incorporated within the linear parkway. An on-site neighborhood park (approximately 5 acres) will terminate the linear parkway to the south.

Parkland areas are also provided intermittently along the linear parkway. These parkland areas will provide additional recreational/open space, while serving as storm water detention areas. Visual and physical ease of access between the recreational/open space system and adjacent neighborhoods and roadways is a central component of the park design. Pedestrian safety through the use of appropriate landscaping and lighting techniques will also be a primary consideration in park system design. Section 6.0, Community Design, provides policies and supporting illustrations to guide the design of the recreational/open space system.

FIGURE 2.2

Recreational/Open Space System

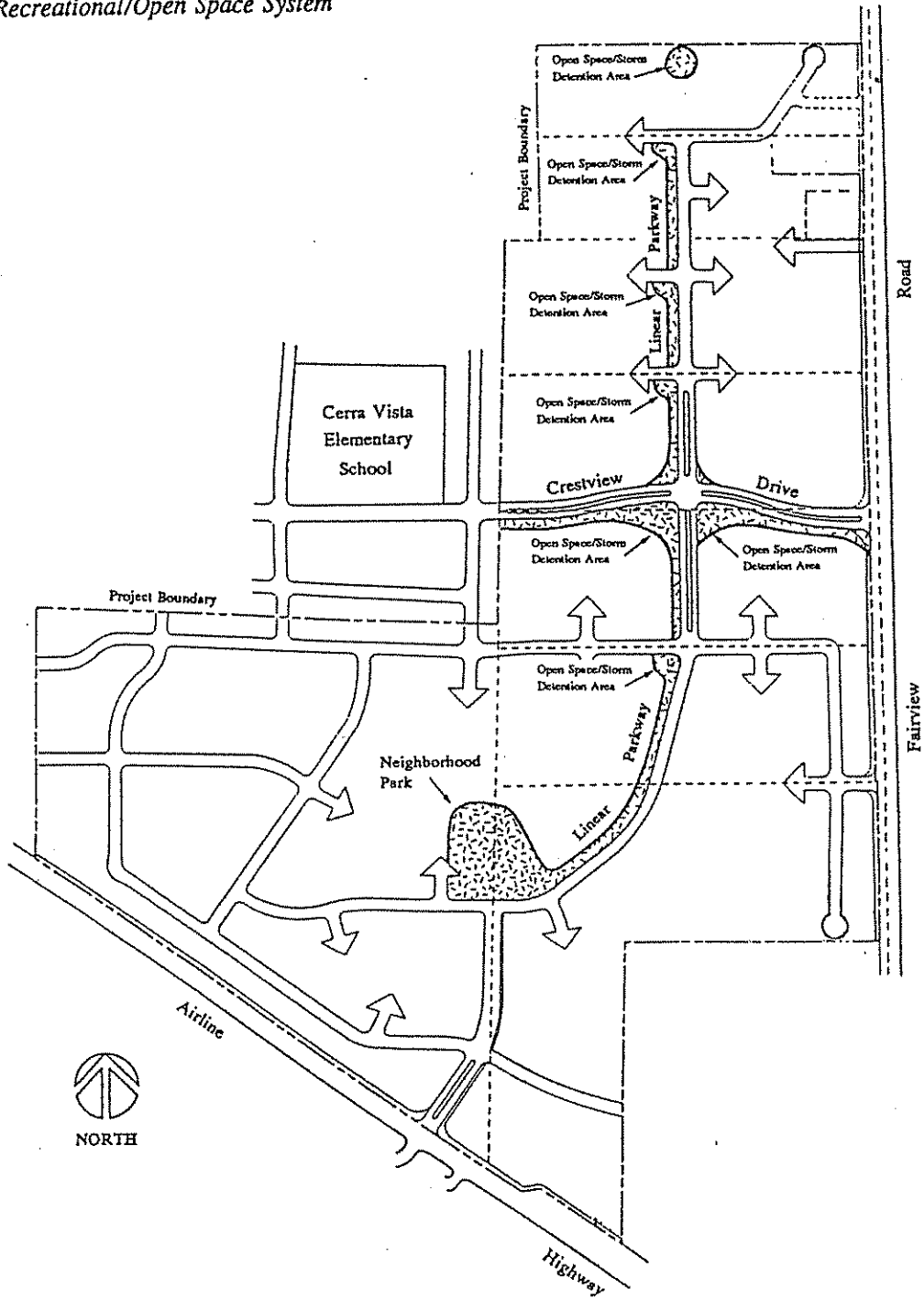
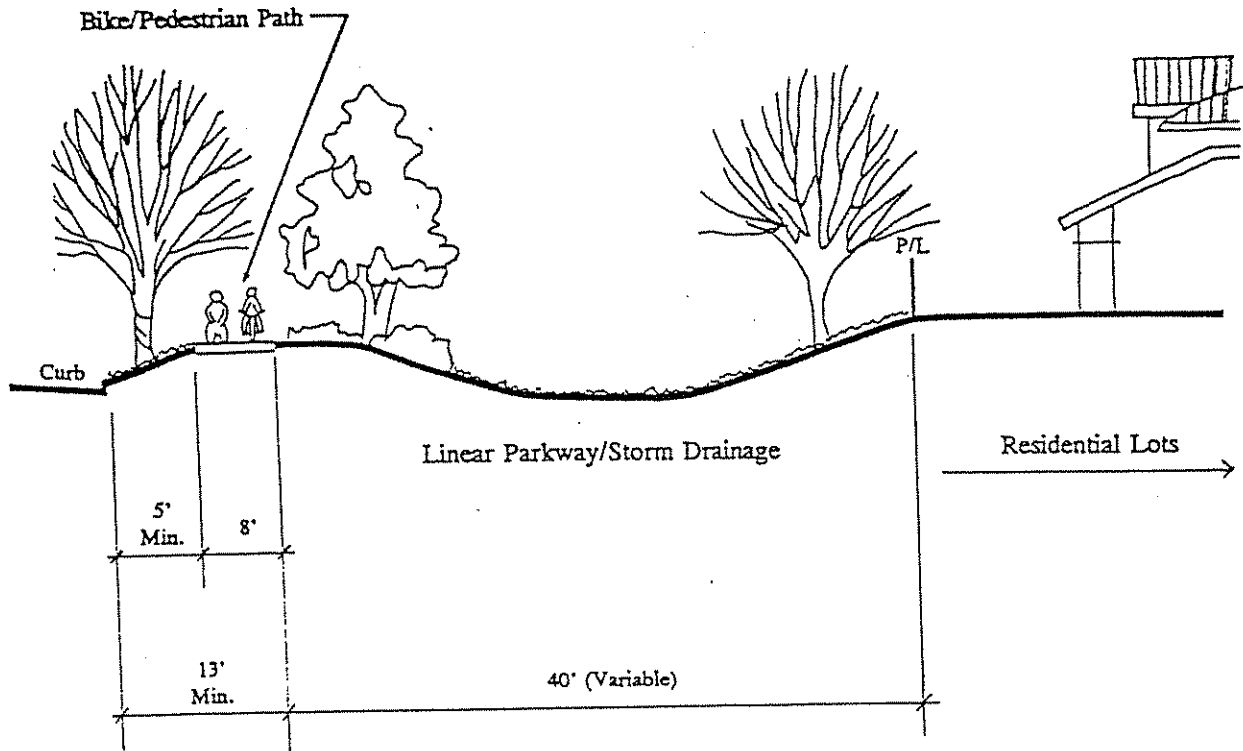


FIGURE 2.3

Linear Parkway



A Lighting and Landscaping Maintenance District will be established to ensure that the Specific Plan parklands and landscape easements are sustained into the future.

SECTION 3.0

HOUSING

3.1 HOUSING MARKET CONDITIONS

Existing Population and Housing

The City of Hollister and San Benito County have undergone steady population and housing growth during the past 20 years. According to U.S. Census estimates, the population of San Benito County was 36,657 on January 1, 1990, a 47 percent increase over the 1980 population of 25,005. Hollister's population increased 67 percent to 19,212 over the same period. Table 3.1 shows this population growth for Hollister and San Benito from 1970 to 1990.

TABLE 3.1 - POPULATION GROWTH 1970 - 1990

	<u>1970</u>	<u>1980</u>	<u>1990</u>
SAN BENITO COUNTY	18,226	25,005	36,697
- Average Annual Growth Rate		3.7%	4.7%
CITY OF HOLLISTER	7,663	11,488	19,212
- Average Annual Growth Rate		5.0%	6.7%
UNINCORPORATED COUNTY	9,399	12,241	15,915
- Average Annual Growth Rate		3.0%	3.0%

Source: U.S. Bureau of the Census.

The number of housing units in San Benito County increased from 8,738 in 1980 to 12,230 as of January 1, 1990, a 40 percent increase (U.S. Census).

Income

Family income and housing costs are generally combined to determine housing affordability. In San Benito County, the median income in 1980 for a family of four was \$19,000. In May, 1993, the median was \$43,700 according to the State Department of Housing and Community Development (HCD). On the average, family incomes thus increased about 130 percent during this period.

State and federal agencies further classify families into four income categories: Very Low, Low, Moderate and Above Moderate. The four categories are defined as follows:

TABLE 3.2 - INCOME CLASSIFICATIONS BASED ON MEDIAN INCOME

<u>Income Category</u>	<u>Household Income as a % of Median County Income</u>
Very Low Income	less than 50%
Low Income	50% to 80%
Moderate Income	80% to 120%
Above Moderate Income	more than 120%

Using the foregoing definitions, four-person families would be classified as follows, according to their incomes as of May, 1993.

TABLE 3.3 - INCOME RANGES BASED ON INCOME CLASSIFICATIONS

<u>Income Category</u>	<u>Annual Income</u>	<u>Monthly Income</u>
Very Low (< 50%)	\$21,850 or less	\$1,821 or less
Low (50% to 80%)	\$34,950	\$2,912
Moderate (80% to 120%)	\$43,700	\$3,641
Above Moderate (> 120%)	\$52,450 or more	\$4,370 or more

3.2 AFFORDABLE HOUSING PROGRAM

The purpose of the Affordable* Housing Program described herein is to encourage diverse housing types in the West Fairview Road Specific Plan area that provide a full range of housing costs. In general, it is assumed that higher residential densities equate to lower housing prices. Therefore, housing policy addressing affordability is considered to be directly related to residential density. Provisions of a mix of housing types is considered an important factor in assuring housing affordability for a full range of household income groups.

The Affordable Housing Program for the West Fairview Road Specific Plan also includes housing affordable to low and moderate income households. Using current State standards, housing is generally considered to be affordable when a household pays no more than 30 percent of its gross monthly income for housing, including utilities.

Affordable Housing Program Policies

The primary purpose of the following Affordable Housing Program policies is to provide housing within the Specific Plan area affordable to the citizens of the City of Hollister and San Benito County. The design criteria defined in Section 6.0, Community Design, will ensure that architectural design and treatment of affordable units is visually compatible with market rate units.

Goal: Housing affordable to a wide range of income groups.

Objective 1: A housing program for the Specific Plan area that ensures provision of housing affordable to low and moderate income groups.

Policy 3.1.1 Based on a density of 5.4 dwelling units per gross acre, a total of ten (10) percent of all dwelling units constructed shall be available to the public at affordable rates.

Actions: a. The City shall require ten (10) percent of all dwellings units constructed within the Specific Plan area to be subject to affordable housing rate standard set forth below at the time of sale.

* Affordable housing as defined in this plan includes housing that is affordable to low and moderate income groups.

Percent of Dwelling Units

Affordable to those Earning the Following Percentage of Median County Income

3%

50% to 79%

7%

80% to 120%

- b. The City shall select an affordable housing agency to administer the sale and management of affordable housing units.

Policy 3.1.2 Those citizens who have resided in the City of Hollister or San Benito County for five years or more will have first priority obtaining affordable housing units, unless otherwise determined by the City Housing Authority or contract agency.

Policy 3.1.3 Each property owner and successor identified in this Plan shall provide their proportionate share of affordable housing as defined by Policy 3.1.1.

- Action: a. A proportionate share of affordable units shall be provided in each development phase. Timing of affordable unit construction within each phase buildout shall be at the discretion of the individual property owners, unless otherwise mutually agreed upon by the City and property owner.

Policy 3.1.4 Affordable dwelling units will be offered for sale to qualified buyers for a 180 day period. If a qualified buyer is not located during that period, the unit will revert back to ownership by the developer to be sold at market rate.

Policy 3.1.5 Distribution of the affordable units throughout the Plan area is encouraged.

Policy 3.1.6 Affordable unit lot configurations, building height and coverage limitations, and parking requirements shall adhere to the Development Standards established in Section 6.3 and to other applicable City zoning regulations.

SECTION 4.0

CIRCULATION

4.1 EXISTING CONTEXT

Two major roadways create direct frontage to the West Fairview Specific Plan area: Fairview Road and Airline Highway (Highway 25). Fairview Road travels in a north-south direction, adjacent to the eastern boundary of the project area. Presently designated as a two-lane rural County road, Fairview Road has a 55 mile per hour speed limit. 2,500 average daily vehicle trips currently travel on the segment of Fairview Road adjacent to the Specific Plan site. No left turn channelization currently exists on Fairview Road within the Specific Plan vicinity. The roadway presently operates at an A level of service (LOS). LOS is a qualitative description of congestion and driver comfort, with LOS A indicating free flowing, uncongested conditions and LOS F representing stop and go, extremely congested conditions.

Airline Highway (State Route 25) runs parallel to the southwest border of the Plan area. In the project vicinity, Airline Highway is a two-lane rural highway with left turn lanes. The roadway operates at a 55 mile per hour speed limit. 6,000 average daily vehicle trips presently travel on the segment of Airline Highway adjacent to the project site, at a current LOS B.

4.2 TRANSPORTATION POLICIES

As required by the State of California Office of Planning and Research, goals, objectives, policies and implementation actions pertaining to transportation are contained below. The goals and objectives address circulation improvements, and transit, bicycle and pedestrian facilities for the Specific Plan area. All policy statements contained in this Specific Plan are based on, and are consistent with, the goals and policies defined in the City of Hollister General Plan. Implementation actions follow policies where appropriate.

The goals, objectives, policies and implementation actions presented below serve as a basis for the planned circulation improvements illustrated in Figures 4.1 through 4.7. The circulation improvements set forth in this Plan are limited to the major circulation elements needed for anticipated future development. More detailed circulation planning for specific development proposals will need to be undertaken at the time parcel specific projects are proposed.

The following goals, objectives, policies and implementation actions are expanded upon by a subsequent narrative description of proposed transportation improvements and supporting illustrations.

Goal: Comprehensive circulation planning and improvements for future development in the West Fairview Specific Plan area, including planned circulation routes for vehicles, bicycles and pedestrians.

Objective 1: Access to the West Fairview area and circulation thereon that is safe and convenient for pedestrians, cyclists and vehicles.

Policy 4.1.1: Roadways and intersections serving the Specific Plan area shall be properly designed to accommodate project vehicle trips.

- Actions:
- a. The owners and the City shall enter into a right-of-way and deferred improvement agreement to provide improvements to Airline Highway and Fairview Road. These planned improvements, illustrated in Figures 4.1 and 4.3, shall be constructed prior or concurrent to development of adjacent properties in the Project area.
 - b. The owners shall provide a pro-rata share of the off-site roadway and intersection improvements required to accommodate project vehicle trips.
 - c. Final designs for all circulation improvements shall be subject to review and approval by the City Public Works Director.

Policy 4.1.2: Roadways within the Specific Plan area shall provide for safe and efficient vehicular movement.

- Actions:
- a. Ingress, egress and traffic circulation throughout the West Fairview area shall comply with City standards and to the standards set forth in this Plan.
 - b. Traffic signals and stop signs shall be provided where warranted, within the guidelines of the Caltrans Traffic Manual.
 - c. Access along Airline Highway and Fairview Road shall be limited to the locations indicated on Figure 4.4.
 - d. Local streets within neighborhoods shall be designed to reflect a modified-grid pattern.

- e. The number of curb cuts connecting the site with collector or arterial streets should be minimized.
- f. Internal traffic circulation between Planning Area A and lots in Planning Area B shall be coordinated. Mutual access easements should be used to minimize paved areas and curb cuts.
- g. Local streets within neighborhoods shall be designed to discourage through traffic via a clear hierarchy of streets.
- h. The City shall prohibit parking on Airline Highway and Fairview Road, other than emergency parking, and signage to that effect shall be placed by the City at appropriate intervals along the roadways.
- i. Final designs for all circulation improvements shall be reviewed by the City Engineering Director to ensure compliance with Actions a. through h. above.

Policy 4.1.3: The alignment and design of project roadways shall be consistent with the recommended improvements set forth in the City of Hollister Draft Traffic and Circulation Master Plan for the West Fairview area.

- Action: a. Final designs for all circulation improvements shall be reviewed by the City Engineering Director to ensure compliance with City of Hollister Draft Traffic and Circulation Master Plan for the West Fairview area.

Policy 4.1.4: Project roadways shall be designed and constructed to meet City roadway standards to ensure that adequate fire protection services can be provided.

- Action: a. Final designs for all circulation improvements shall be reviewed by the City Engineering Director and Fire Chief to ensure compliance with City roadway standards to ensure that adequate fire protection services can be provided.

Policy 4.1.5: All project roadways shall conform to the City of Hollister Standard Specifications and Details.

- Action: a. Final designs for all circulation improvements shall be reviewed by the City Engineering Director to ensure conformance with the City of Hollister Standard

Objective 2: Use of public transit by West Fairview area residents.

Policy 4.2.1: Alternatives to the automobile as a means of transportation shall be encouraged. Public transportation services, such as those provided by SANTRANS, should be expanded to the Specific Plan area as development occurs.

Action: a. Right-of-way for future bus turnouts shall be allocated subject to the review and approval of SANTRANS officials and the City Engineering Director.

Objective 3: To establish and maintain a bicycle route system which services commuter, school and recreational bicycling needs.

Policy 4.3.1: Bicycle paths shall be provided for transportation and recreational purposes, consistent with the County of San Benito Bikeway Plan and City of Hollister Parks and Recreation Master Plan.

Actions: a. As illustrated in Figure 4.6, Bikeway System, two-way bike paths shall be constructed within linear parkways. In addition, Class I and Class II bike paths shall be constructed where indicated on Figure 4.6.

b. Final designs for all bikeway improvements shall be reviewed by the City Engineering Director to ensure conformance with the County of San Benito Bikeway Plan, and the City of Hollister Parks and Recreation Master Plan.

Policy 4.3.2 Bikeways within the Specific Plan area shall be designed to allow safe and convenient bicycling.

Actions: a. The Class I bike path and bikeways within the linear park shall be a minimum of eight (8) feet in width, and shall not meander more than the distance of its width over a 100-foot run.

b. Except for emergencies, all street parking along designated bicycle lanes shall be prohibited in order to reduce the conflict between bicycles and other modes of travel.

c. The City shall erect signs, per City standards, prohibiting street parking along designated bicycle lanes.

Policy 4.3.3: All bikeways within the Specific Plan area shall be maintained by a Lighting and Landscape Maintenance District.

- Action: a. The City of Hollister and project developers shall establish a Lighting and Landscaping Maintenance District, and shall enter into a Lighting and Landscaping Maintenance Agreement, prior to Tentative Map approval.

Objective 4: Facilities for safe and pleasant pedestrian travel.

Policy 4.4.1: Sidewalks shall be provided in appropriate areas to ensure safe pedestrian travel.

- Actions: a. A pedestrian walkway shall be incorporated into the linear park system.
- b. Unless illustrated otherwise, project roadways shall be developed with a 5-foot sidewalks, on both sides of the street, as defined by the City of Hollister Standards Specifications and Details. (See Figure 4.5.)
- c. Final designs for pedestrian circulation improvements, including pedestrian paths, crossings and street lighting shall be subject to the review and approval of the City Public Works Director.

Policy 4.4.2: Street lighting shall be provided for pedestrian safety.

- Action: a. The siting and design of project area street lighting shall be in accordance with the City of Hollister Standard Specifications and Details.

Policy 4.4.3: Adequate provision shall be made for pedestrian crossings at roadway intersections and other selected points.

Policy 4.4.4: Barrier free access shall be provided to park system components.

- Action: a. Pedestrian crossings at roadway intersections and other appropriate locations shall be ramped in accordance with the City of Hollister Standard Specifications and Details (see also Appendix A, Design Standards).

4.3 CIRCULATION IMPROVEMENTS

Development of the West Fairview Specific Plan area will result in the construction of a maximum of 1594 dwelling units. Units constructed may range from single family units to multiple family units and senior housing. The number of vehicle trips generated will be partially dependent on the mix of housing types. For example, a single-family dwelling unit would likely generate substantially more vehicle trips than a senior housing unit.

The assessment of project generated vehicle trips was based on 1500 single family dwelling units. Assuming a trip generation factor of 10 vehicle trips per dwelling unit, development of the Specific Plan area with 1500 single family dwelling units will result in 15,000 average daily vehicle trips. Seventy percent of these trips will travel west on Airline Highway towards Hollister, 20 percent will travel north on Fairview Road, 5 percent will travel south on Enterprise Road, and 5 percent will travel east on Airline Highway.

The following discussion summarizes the direct, indirect, and internal circulation improvements necessitated by the development of the West Fairview Road Specific Plan area.

Direct Circulation Improvements

To accommodate the vehicle trips generated by development of the West Fairview area three major roadway improvements are proposed in this Circulation Plan:

- Airline Highway Widening
- Fairview Road Widening
- Crestview Drive Construction

These improvements are illustrated in Figures 4.1 through 4.5 and are described below.

Airline Highway Widening

Existing Conditions

Assuming that the existing roadway configuration within the City of Hollister and its surrounding vicinity is still in place at buildout of the Specific Plan area, project generated vehicle trips will affect Airline Highway as follows: Project generated traffic will increase the average daily vehicle trips on the portion of Airline Highway adjacent to the site from 6,000 to 16,528; a 175 percent increase. Short-term cumulative development, including the project, will increase the average daily vehicle trips on this roadway segment to 23,642, while long-term cumulative development will increase the average daily vehicle trips to 37,020.

In the project vicinity, Airline Highway is a two-lane rural highway with left turn lanes. Under this configuration, project generated traffic will decrease the operating LOS of this roadway from B to D. (An LOS of C or better is considered acceptable under City standards.) To

maintain an acceptable LOS, the portion of Airline Highway in the immediate vicinity of the Specific Plan area will require improvement to a four-lane arterial without left turn lanes at major intersections. With improvements, a B LOS would result. Short-term cumulative development, including the project, would require left turn channelization on this segment of Airline Highway to maintain a B LOS. Under the long-term cumulative development including project scenario, to maintain an acceptable LOS the portion of Airline Highway within the immediate vicinity of the Specific Plan area would require improvement to a six-lane arterial with left turn lanes at most intersections.

Future improvements to Airline Highway are illustrated on Figure 4.1. As noted above, improvements shall be required as project, short-term, and long-term development occurs within the City of Hollister and surrounding vicinity. To implement Airline Highway improvements, a right-of-way dedication and deferred improvement agreement shall be established between the West Fairview Road Specific Plan property owners and the City.

FIGURE 4.1

Airline Highway Improvements

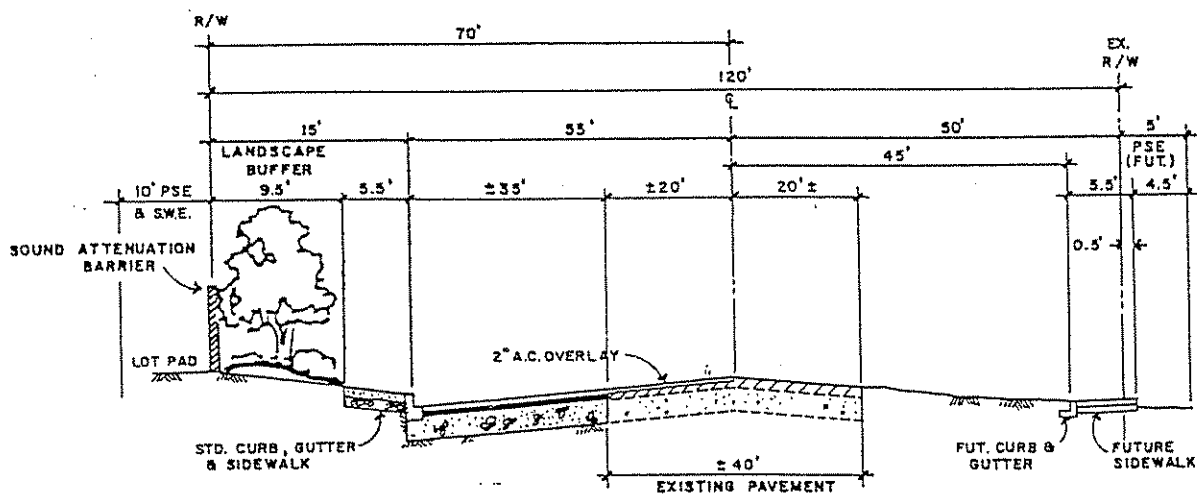
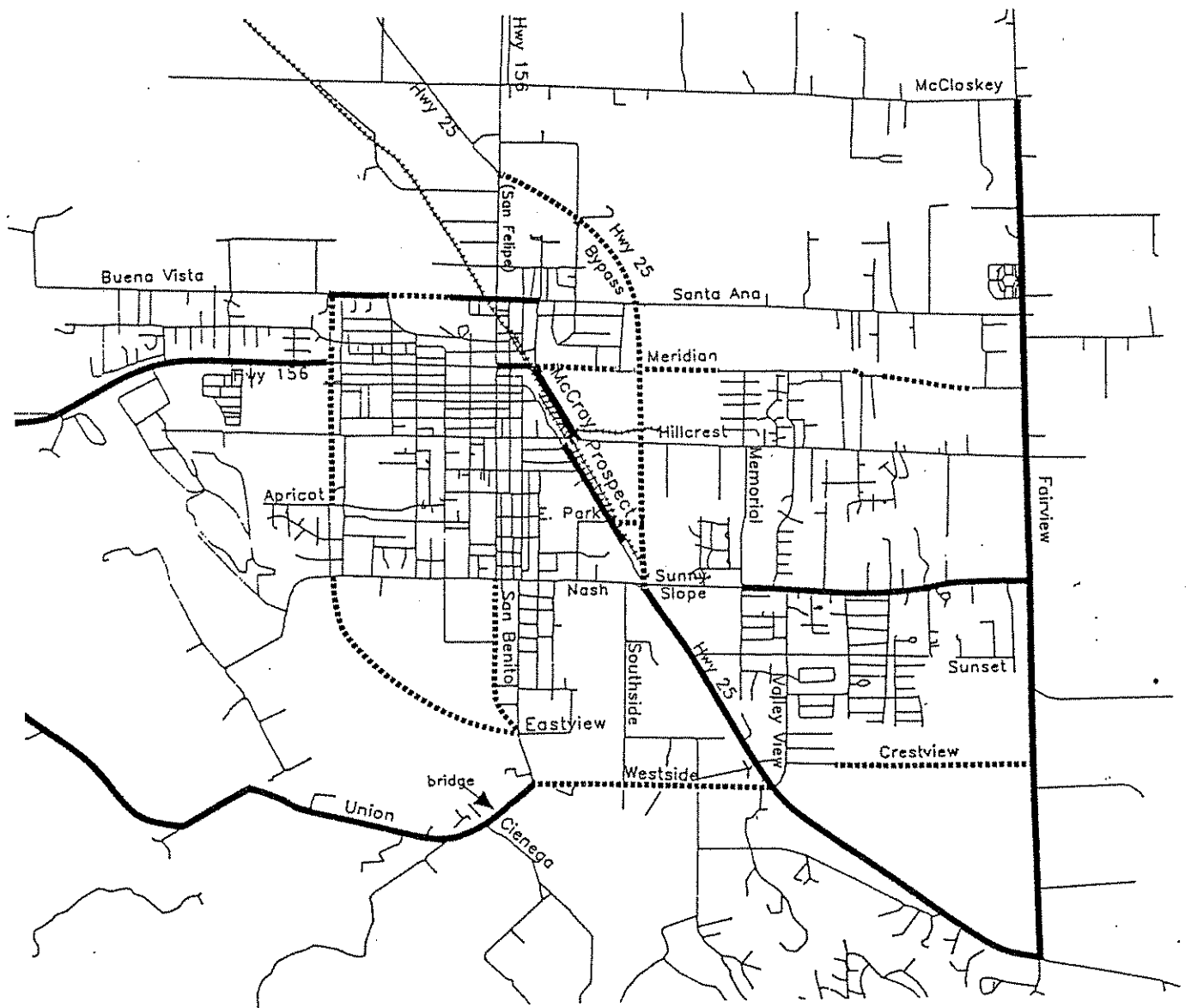


FIGURE 4.2

Future Highway and Roadway Network



- Minor Roads
- New Roads
- Road Widening

Future Conditions

Figure 4.2 illustrates the future highway and roadway networks as currently envisioned by the County of San Benito, City of Hollister and Caltrans. Highway improvements include the realignment of Highway 152 and the construction of the Highway 156 and Highway 25 bypasses and San Benito Street Extension/Westside Boulevard project. Various major roadway improvements throughout the City are also proposed, including the construction of Crestview Drive through the Specific Plan area.

With these highway and roadway improvements in place, the average daily vehicle trips on Airline Highway within the vicinity of the Specific Plan area will be altered from that discussed above. For example, under the existing plus project scenario, the average daily vehicle trips on the portion of Airline Highway adjacent to the site would decrease from 16,528 to 12,396; under the short-term cumulative plus project scenario, vehicle trips would decrease from 23,642 to 17,731; and under the long-term cumulative plus project scenario, vehicle trips would decrease from 37,020 to 27,765. Given the noted decrease in vehicle trips, the LOS of roadway improvements recommended under existing conditions to accommodate project, and short- and long-term cumulative development would improve as future highway and roadway network improvements are made.

Fairview Road Widening

Existing Conditions

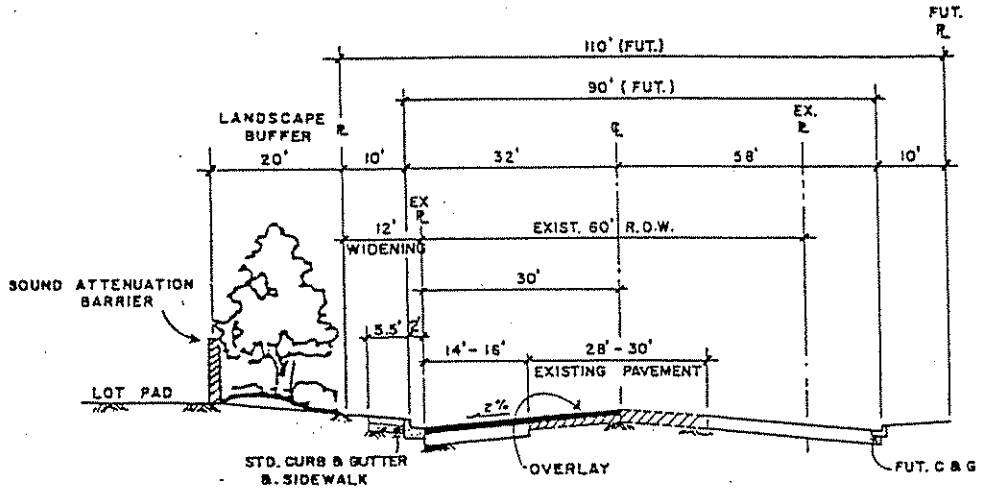
Assuming that the existing roadway configuration within the City of Hollister and its surrounding vicinity is still in place at buildout of the Specific Plan area, project generated vehicle trips will affect Fairview Road as follows: Project generated traffic will increase the average daily vehicle trips on the portion of Fairview Road adjacent to the site from 2,500 to 5,508; a 120 percent increase. Short-term cumulative development, including the project, will increase the average daily vehicle trips on this roadway segment to 7,764, while long-term cumulative development will increase the average daily vehicle trips to 15,150.

In the project vicinity, Fairview Road is a two-lane county road with no left turn lanes. Under this configuration, project generated traffic will decrease the operating LOS of this roadway from A to B. With short-term cumulative development traffic, including the project, a B LOS would be maintained. Under the long-term cumulative development including project scenario, to maintain an acceptable LOS (C or better) the portion of Fairview Road within the immediate vicinity of the Specific Plan area would require improvement to a four-lane arterial with left turn lanes at most intersections.

Future improvements to Fairview Road are illustrated on Figure 4.3. As noted above, improvements shall be required as project, short-term, and long-term development occurs within the City of Hollister and surrounding vicinity. To implement Fairview Road improvements, a right-of-way dedication and deferred improvement agreement shall be established between the West Fairview Road Specific Plan property owners and the City.

FIGURE 4.3

Fairview Road Improvements



Future Conditions

With the highway and roadway improvements previously described in place, the average daily vehicle trips on Fairview Road within the vicinity of the Specific Plan area will be altered from that discussed above. For example, under the existing plus project scenario, the average daily vehicle trips on the portion of Fairview Road adjacent to the site would increase from 5,508 to 5,962; under the short-term cumulative plus project scenario, vehicle trips would increase from 7,764 to 9,118; and under the long-term cumulative plus project scenario, vehicle trips would increase from 15,150 to 16,328. Given the noted increase in vehicle trips, the LOS of Fairview Road along the project frontage would remain a B LOS under the existing plus project scenario; decrease from a B to a C LOS (considered acceptable) under the short-term cumulative plus project scenario, and remain an A LOS under the long-term cumulative plus project scenario, assuming Fairview Road is improved to four lanes with left turn channelization.

Crestview Drive Construction

The City of Hollister Draft Traffic and Circulation Master Plan for the West Fairview area calls for the extension of Crestview Drive from the Airline Highway/Valley View Road intersection to Fairview Road. The roadway will relieve traffic from Airline Highway between Valley View

Road and Fairview Road, and Sunnyslope Road between Airline Highway and Fairview Road.

The alignment of Crestview Drive through the Specific Plan area is illustrated on Figure 4.4. The roadway shall be constructed as a four lane divided arterial, with left turn lanes. Construction of Crestview Drive will occur in conjunction with the development of parcels A5 and A6 (see Figure 1.4).

Indirect Circulation Improvements

Many roadway segments and intersections within the City of Hollister and surrounding areas are currently operating at unacceptable service levels under existing conditions. Vehicle trips generated by Specific Plan residents will affect existing and proposed roadway segments and intersections within the project vicinity and downstream areas. As development of the Specific Plan area occurs, in conjunction with proposed short- and long-term cumulative development, circulation improvements will be required.

To finance future improvement costs, traffic impact fees collected from development of the Specific Plan area will be applied to those roadway segments and intersections warranting improvement. Pro-rata funding generated through special assessment and/or developer contributions may also be required.

Internal Roadway System

Figures 4.4 and 4.5 illustrate the internal roadway system for the West Fairview Specific Plan area. The primary circulation components include Crestview Drive, a City planned arterial, and a north-south traveling collector. As presented on Figures 4.4 and 4.5, Specific Plan arterials and collectors will have either 84 or 64 foot right-of-ways, depending on location. Local neighborhood streets will have 60 foot right-of-ways for roadways accessing 50 dwelling units or more, and 50 foot right-of-ways for roadways accessing less than 50 units.

The circulation system provides safe and pleasant access to the future neighborhoods within the Specific Plan area. For example, no driveway access will be allowed along project arterials and collectors, or Fairview Road and Airline Highway. In addition, the roadway system was designed in conjunction with the linear and neighborhood park system; preserving the visual quality and sense of open space along the project arterial and north-south collector to the maximum extent feasible. Other features of the internal roadway system are summarized below:

- Access along Airline Highway and Fairview Road is limited to the locations indicated on Figure 4.4.
- Medians shall be provided at primary project entrances and arterial intersections to enhance their aesthetic appeal.

FIGURE 4.4

Internal Roadway System

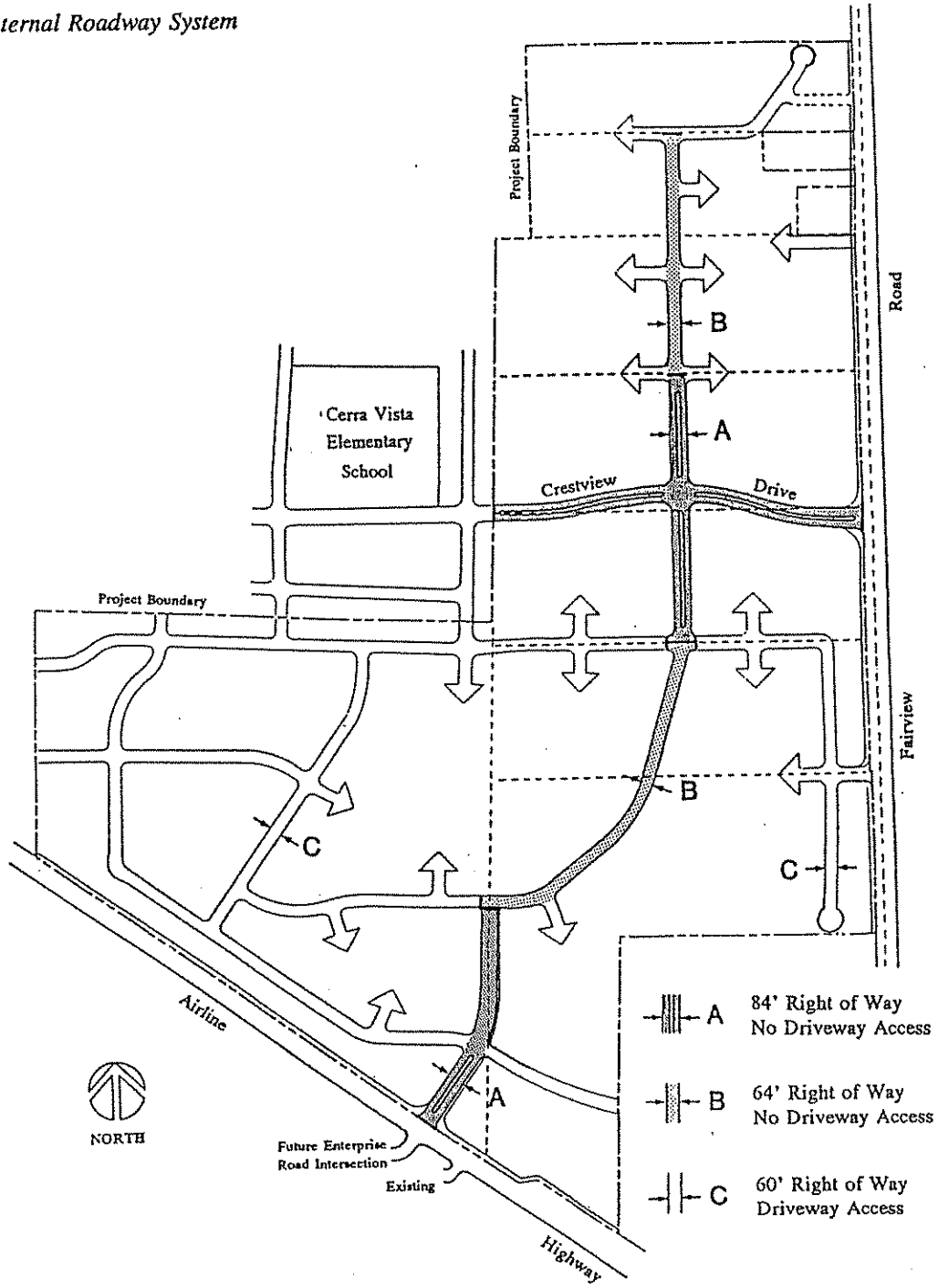
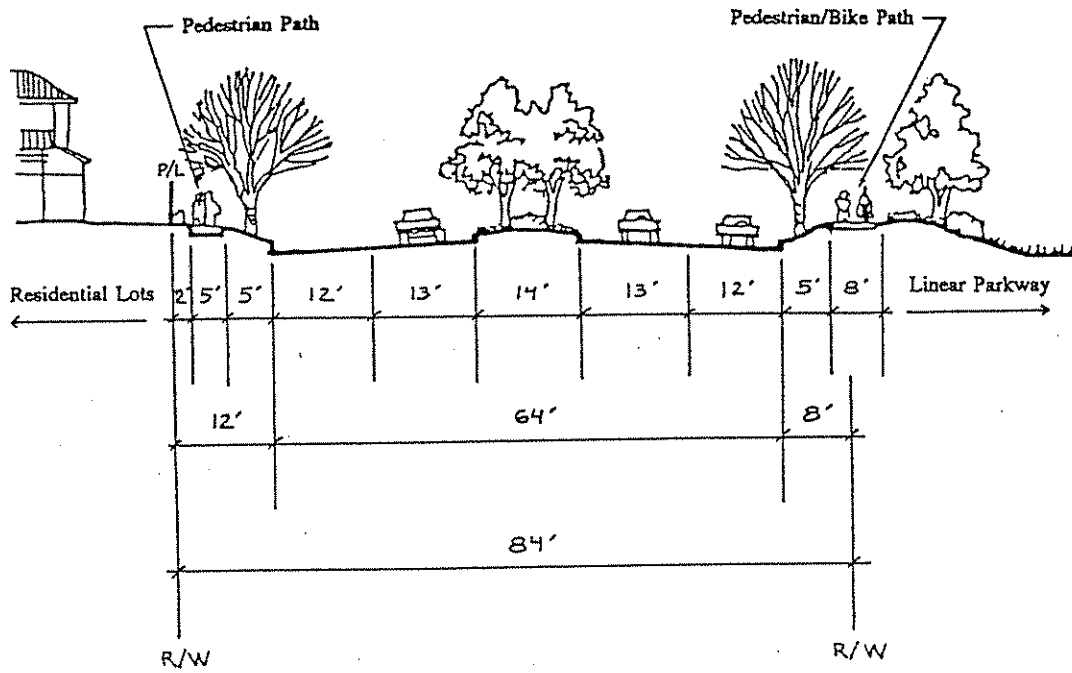
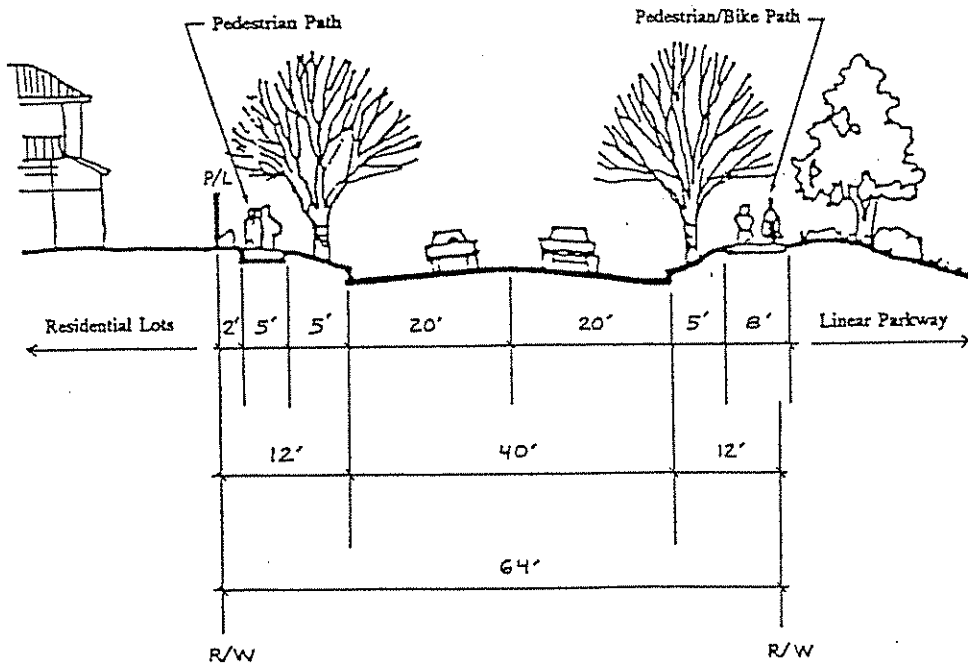


FIGURE 4.5

Internal Roadway Cross Sections



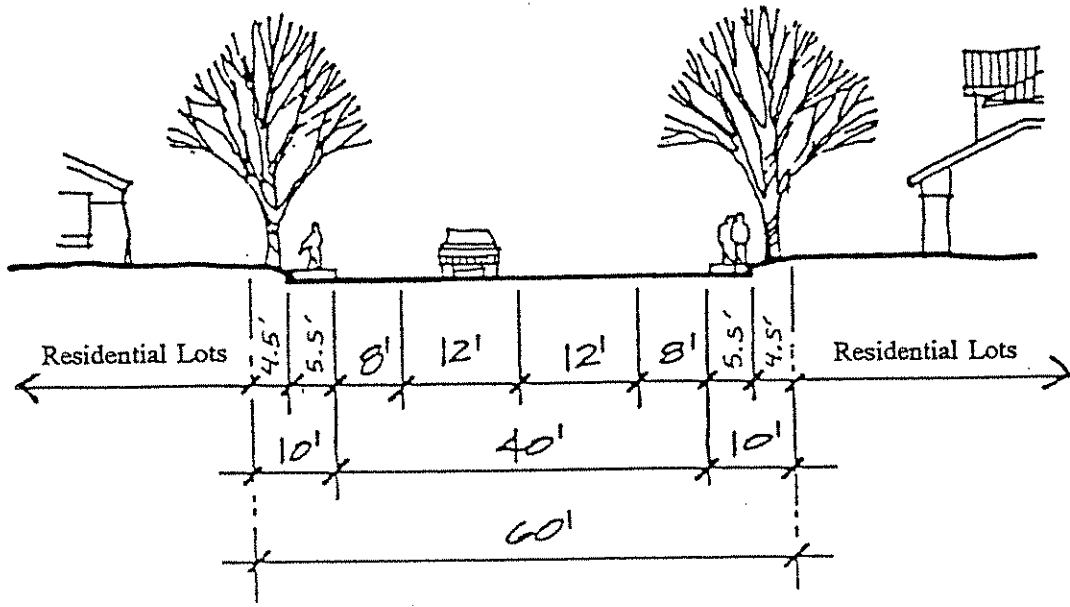
84' Right-of Way, No Driveway Access (NTS)



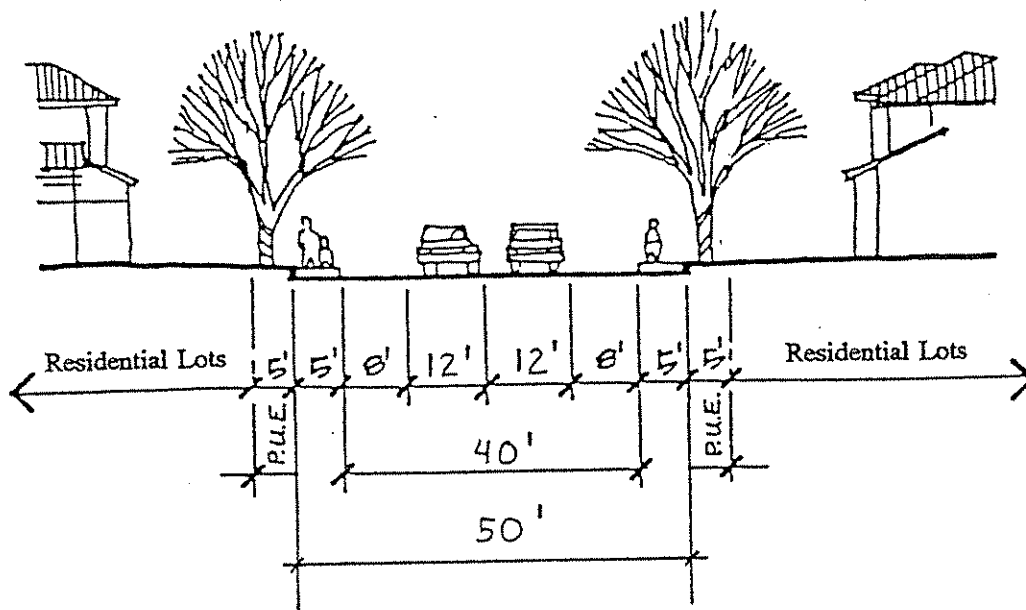
64' Right-of-Way, No Driveway Access (NTS)

FIGURE 4.5

Internal Roadway Cross Sections



60' Right-of-Way, 50 - 200 Dwelling Units (NTS)



50' Right-of-Way, < 50 Dwelling Units (NTS)

- To encourage pedestrian safety, local streets within neighborhoods shall be designed to discourage through traffic; pedestrian walkways/sidewalks shall be provided within the linear parkways and along all project roadways; and bikeways shall be provided within the linear parkway and along all project arterial and collector roadways.
- To promote the compatibility of future Specific Plan neighborhoods with existing Hollister neighborhoods, internal neighborhood roadways shall be designed as modified-grid systems.
- Traffic signals and stop signs shall be provided where warranted, within the guidelines of the Caltrans Traffic Manual.

4.4 PUBLIC TRANSIT

Public transit service is currently provided in Hollister and portions of San Benito County by San Benito County Transit (SANTRANS). Due to low ridership, terms of a new 3-year contract, effective July 1, 1990, eliminated the fixed-route system which consisted of a single 25-passenger bus on a 70-minute loop throughout Hollister. Public transit service in the County and City is currently limited to Dial-a-Ride, plus scheduled service to Gavilan College in Gilroy during the school year.

Development within the Specific Plan area will result in an increased demand for Dial-a-Ride service. Despite the curtailment of fixed-route bus service until 1993, the potential for such service at some time in the future still exists. Right-of-ways for future bus turnouts shall be allocated within the Specific Plan area where deemed necessary by SANTRANS officials and the City Public Works Director.

4.5 BICYCLE FACILITIES

To enhance the transportation and recreational needs of the Specific Plan area, a bicycle path system shall be provided. As illustrated on Figures 4.6 and 4.7, the proposed bikeway system consists of a bikeway throughout the linear park, a Class I bike path at the project entrance on Airline Highway, and Class II paths contiguous to the landscape corridors along Airline Highway and Fairview Road.

Bikeways shall be designed to allow safe and convenient bicycling. Bikeways within the linear parkway will be paved, off-street pathways that will be shared with pedestrians. The Class I bike path and bikeways within the linear park shall be a minimum of eight (8) feet in width, and shall not meander so greatly that the safe and convenient movement of cycle traffic is inhibited. The location of Class II bikeways along the project frontages with Airline Highway and Fairview Road is consistent with the San Benito County Bikeway Plan.

FIGURE 4.6

Bikeway System

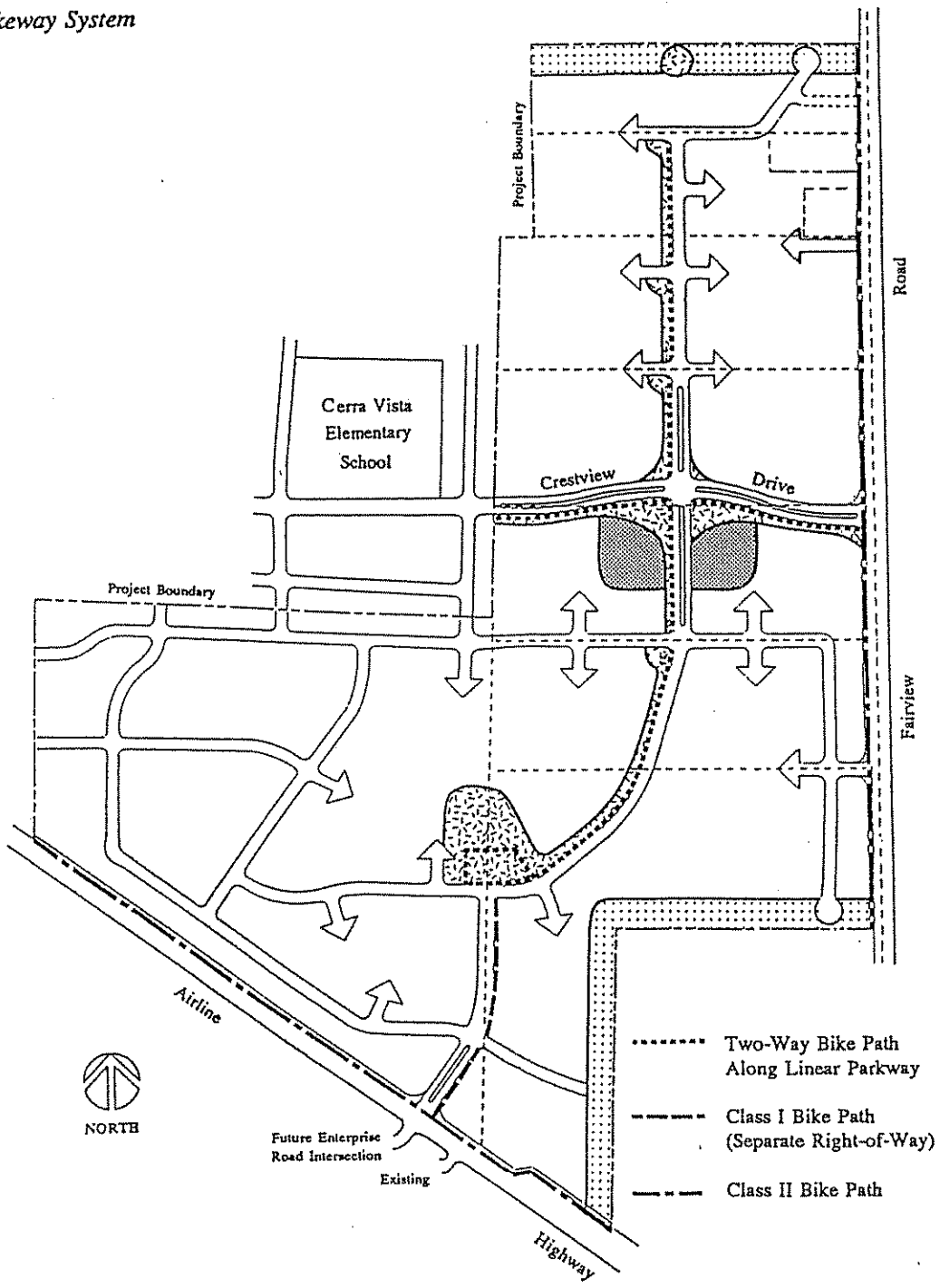
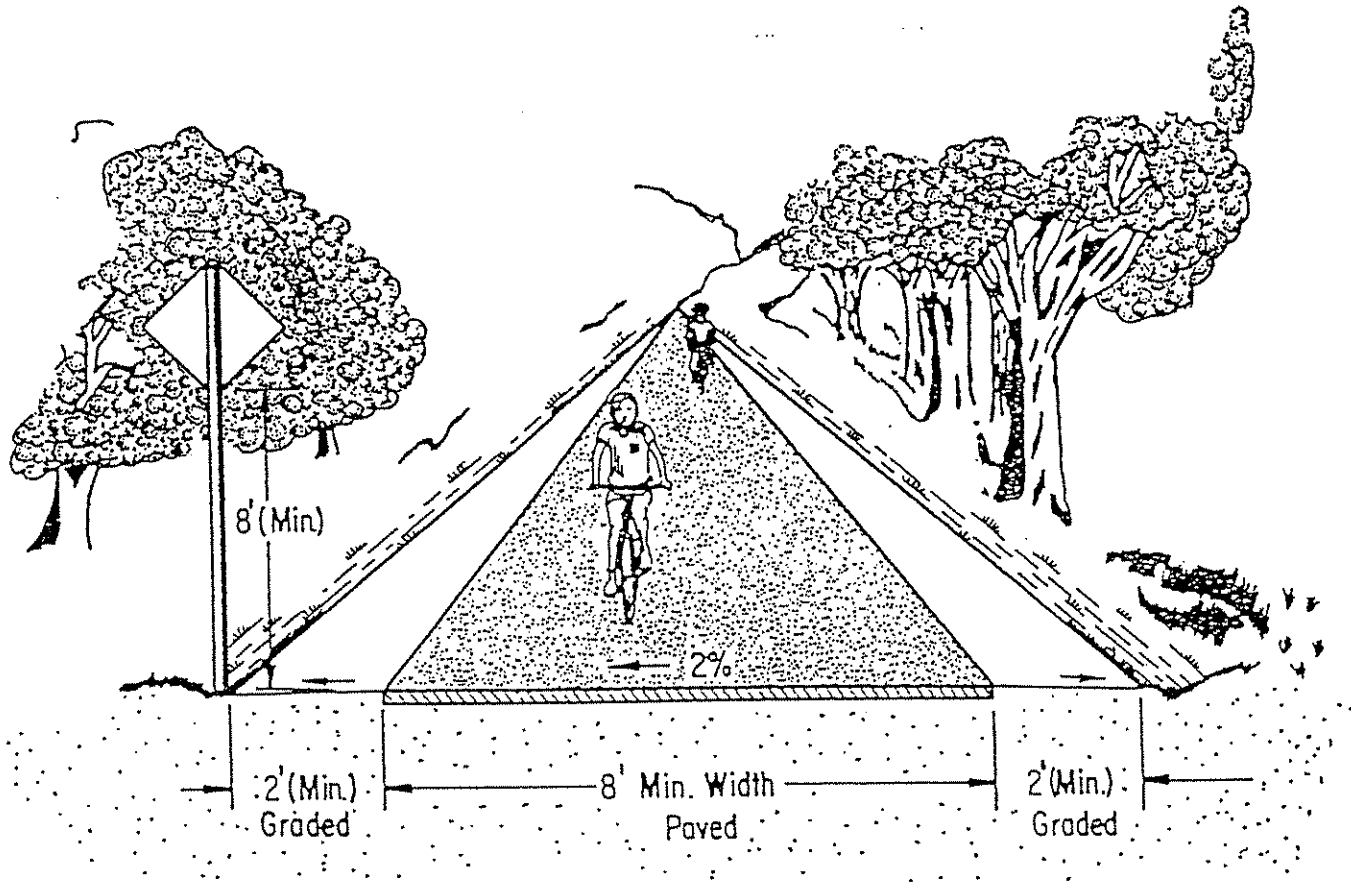


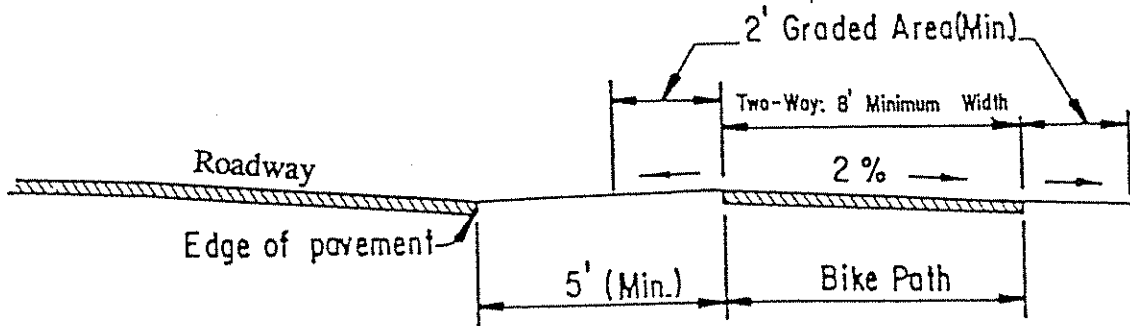
FIGURE 4.7

Bikeway Cross Sections

Two-way Bike Path Along Linear Parkway



Class I Bike Path
Separate (Right-of-Way)



SOURCE: CALTRANS HIGHWAY DESIGN MANUAL;
BIKEWAY PLANNING AND DESIGN.

4.6 PEDESTRIAN FACILITIES

To encourage pedestrian foot travel an extensive pathway/sidewalk system is proposed for the Specific Plan area. As illustrated on Figure 4.5, sidewalks are planned along both sides of all local streets. Collector and arterial roadways shall contain sidewalks on the side of the street opposite to the linear parkway. The Class I bikeway within the linear parkway shall also serve as a pedestrian pathway. Adequate provision shall be made for pedestrian crossings at roadway intersections and other selected points. Street lighting shall be provided for pedestrian safety.

SECTION 5.0

RESOURCE CONSERVATION

The West Fairview Specific Plan area has historically been used for limited agricultural and grazing activities. Implementation of the Specific Plan will convert this agricultural environment into an urban use.

Human habitation will place demands on community resources, such as potable water. In addition, an increased burden will be placed on other community and regional resources, such as air and water quality and the available energy supply.

5.1 RESOURCE CONSERVATION POLICIES

The primary purpose of this Section of the Specific Plan is to preserve and enhance the integrity of the natural resources in the West Fairview Road area and surrounding region. The goals, objectives, policies and implementation actions set forth below serve as a basis for this primary purpose; hydrology, soil and archaeological resource protection, energy, and flora and fauna are addressed. See Section 2.2 for a discussion of the conservation of open space through the development of a park system.

The following goals, objectives, policies and implementation actions are expanded upon by a subsequent narrative description of community and regional resources and supporting illustrations.

Goal: To provide for the conservation, development and utilization of natural resources.

Objective 1: To preserve and protect the community's natural resources.

Policy 5.1.1: Protect natural drainage and water recharge areas.

- Actions:
- a. The use of impervious materials, especially in recreational/open space and residential areas, shall be minimized.
 - b. On-site storm drainage retention areas, or other water recharge improvements, shall be integrated into the final site designs for individual development proposals to mitigate loss of recharge where feasible.

- c. Sediment traps, evaporation basins, flow reduction devices and other methods to reduce the volume of grease and oil pollutants in street surface runoff shall be installed in the storm drain system as directed by the City Engineering Director.

Policy 5.1.2: Water conservation shall be encouraged.

- Actions:
- a. Water conserving plumbing fixtures, including flow restricting devices, shall be installed in all inhabitable structures.
 - b. Landscape irrigation systems which incorporate water conservation techniques such as low precipitation spray heads and drip irrigation, shall be encouraged.
 - c. Landscaping of park facilities, landscape buffers, and residential areas shall incorporate the use of native, drought-resistant vegetation.
 - d. All streetscape irrigation systems will be designed and installed in accordance with the guidelines and standards contained in the City of Hollister Streetscape Improvement Plans (refer to Appendix A, Design Standards).

Policy 5.1.3: Minimize soil erosion in developed and undeveloped areas.

- Actions:
- a. To reduce wind erosion, graded areas shall be seeded and watered during grading activities.
 - b. Soil exposed during grading operations or left exposed during the rainy season shall be replanted with native compatible, drought-resistant vegetation.

Policy 5.1.4: Archaeological and cultural resources shall be protected.

- Actions:
- a. Where any excavation or grading work results in discovery of cultural, archaeological or anthropological resources or artifacts, all work shall be halted immediately for a distance of 300 feet from the discovery site, a qualified archaeologist shall be consulted for on-site excavation, and the State Historic Preservation Office shall be notified.

- b. Any structures within the Specific Plan area deemed historic shall be studied and photographed to determine their historical significance prior to construction activities.

Policy 5.1.5: All habitable structures shall be designed and oriented to maximize the potential for energy conservation wherever feasible. Such measures shall address, but are not limited to, utilization of solar energy.

Policy 5.1.6: Native vegetation communities shall be conserved where possible for their aesthetic and habitat value.

- Actions:
- a. Encourage use of native vegetation in park and residential landscape areas.
 - b. Final building and landscaping plans for the neighborhood park and linear park shall be subject to review and approval of the City Community Services Director.

Policy 5.1.7: Potential degradation to the region's air quality shall be minimized through the promotion of public transit and bicycle and pedestrian pathway systems.

5.2 HYDROLOGY

San Benito County and the City of Hollister are dependent on their groundwater resources for domestic, commercial, industrial and irrigation uses. The protection of groundwater resources is vital to the economic well being, public health and general welfare of local citizens.

The hydrological components addressed by this section include the protection of natural drainage and water recharge areas, and water quality and conservation. Storm drainage collection and water distribution infrastructure are discussed in Sections 7.3 and 7.4, respectively.

Protection of Natural Drainage and Water Recharge Areas

As discussed in Section 2.2, the Specific Plan land use plan features a linear and neighborhood park system. To encourage the protection of natural drainage and water recharge areas, the park components are designed to compliment these natural hydrological features. Figure 5.1 illustrates the park components with respect to their hydrological function.

The linear park is comprised of two branches: the north-south and the east-west. The north-south branch of the linear park follows the natural drainage swale through the area. While the primary function of this linear park branch is to provide recreational/open space amenities, its secondary role is to function as a storm drainage channeling facility during peak storm events.

Low rainfall in the area and design limit the need for park areas to handle storm drainage to only 1 to 2 days for a 25-year storm. Thus retention areas would be unused in most years, and would have water in them for a brief period during infrequent flood flow events¹. Most of the time they would serve solely as parks. As illustrated on Figure 5.1, larger parkland areas are provided intermittently along the linear parkway. These parkland areas, in conjunction with the linear parkway, will also serve as storm drainage detention basins (in addition to their recreational/open space use). By serving as detention basins, the larger parkland areas of the linear parkway will also function as groundwater recharge areas.

While the linear parkway is intended to channel and detain excess runoff during peak storm events, the areas will be landscaped for recreational/open space uses. The precise locations and sizes of detention basins will be established with the approval of subsequent Tentative Maps.

Water Quality

Development within the Specific Plan area will increase the area of impervious surfaces due to the covering of vacant land with paved streets and sidewalks, and buildings. By applying impervious surfaces to the land, the rate of storm water runoff increases. As runoff travels across the impervious surfaces it collects the urban pollutants on the surface. The degradation of water quality becomes a concern in areas where urban runoff is allowed to enter natural drainage courses. The groundwater resources recharged by, and biological communities residing within, these natural drainage courses become jeopardized.

To mitigate the potential impact of urban runoff on the natural drainage course environments, sediment traps, evaporation basins, and flow reduction devices will be installed in the storm drain system. These methods will reduce the volume of urban pollutants entering the natural drainage system.

Water Conservation

The potential depletion of the groundwater resources due to drought, development, and associated overdraft necessitate water conservation measures. Such measures include the installation of water efficient and flow restrictive plumbing fixtures in all structures. Low precipitation spray heads and drip irrigation methods enhance the water conservation ability of landscape irrigation systems. The installation of water conserving irrigation systems should be encouraged for all yards, parkways, and public landscape areas (buffers, medians, etc.). The use of native, drought-resistant vegetation throughout the Specific Plan area will further enhance water conservation.

¹MacKay and Somps, 1993.

5.3 SOIL PROTECTION

The San Benito County Soil Survey identifies two soil series within the Specific Plan area: Antioch and Rincon. Figure 5.2 illustrates the distribution of these soil series throughout the Plan area. Table 5.1 summarizes the characteristics of each soils type.

As Figure 5.2 illustrates, the majority of the Plan area is comprised of Antioch Loam soils, with slopes ranging from 0 to 9 percent. The remainder of the site contains Rincon Silty Clay Loam, 2 to 9 percent slopes. Depending on slope, the erosion potential of Specific Plan soils ranges from slight to moderate. Given the fairly level nature of the Plan area, the erosion impacts associated with development should be minimal, provided that erosion control measures are adhered to during construction activities.

Table 5.1 also summarizes the agricultural capabilities of the Specific Plan area soils. Class I soils are considered prime agricultural soils. In addition, any soil type with a classification of II is considered to be prime agricultural land if irrigation is available on the site. As illustrated in Figure 5.2, all soil types found on the Specific Plan site have a capability rating of class III or below with the exception of Rincon silty clay loam (RsC), which has a capability rating of II. Since there is no current cultivation of this land and there is no irrigation of the land, portions of the site containing RsC soil are not considered to be prime agricultural soils.

5.4 ARCHAEOLOGICAL RESOURCE PROTECTION

Archaeological Resource Management conducted an archival search of the State records and surface reconnaissance of the Specific Plan area to determine the existence of any potential archaeological or cultural resources. The archival research revealed that there are no recorded archaeological sites in or adjacent to the subject areas. However, traces of prehistoric cultural resources (sandstone mortar fragment) and an older residence (approximately 100 years of age) were noted on the subject area during the surface reconnaissance.

To ensure the protection of cultural resources within the Plan area any earth disturbing construction activities in the area deemed archaeologically sensitive will be monitored by a qualified archaeologist. For those areas not deemed archaeologically sensitive, if excavation or grading work results in the discovery of cultural or archaeological resources or artifacts, all work shall be halted immediately for a distance of 300 feet from the discovery site, a qualified archaeologist shall be consulted for on-site excavation, and the State Historic Preservation Office shall be notified. In addition, the century-old building shall be studied and photographed to determine its history and level of local significance prior to construction activities.

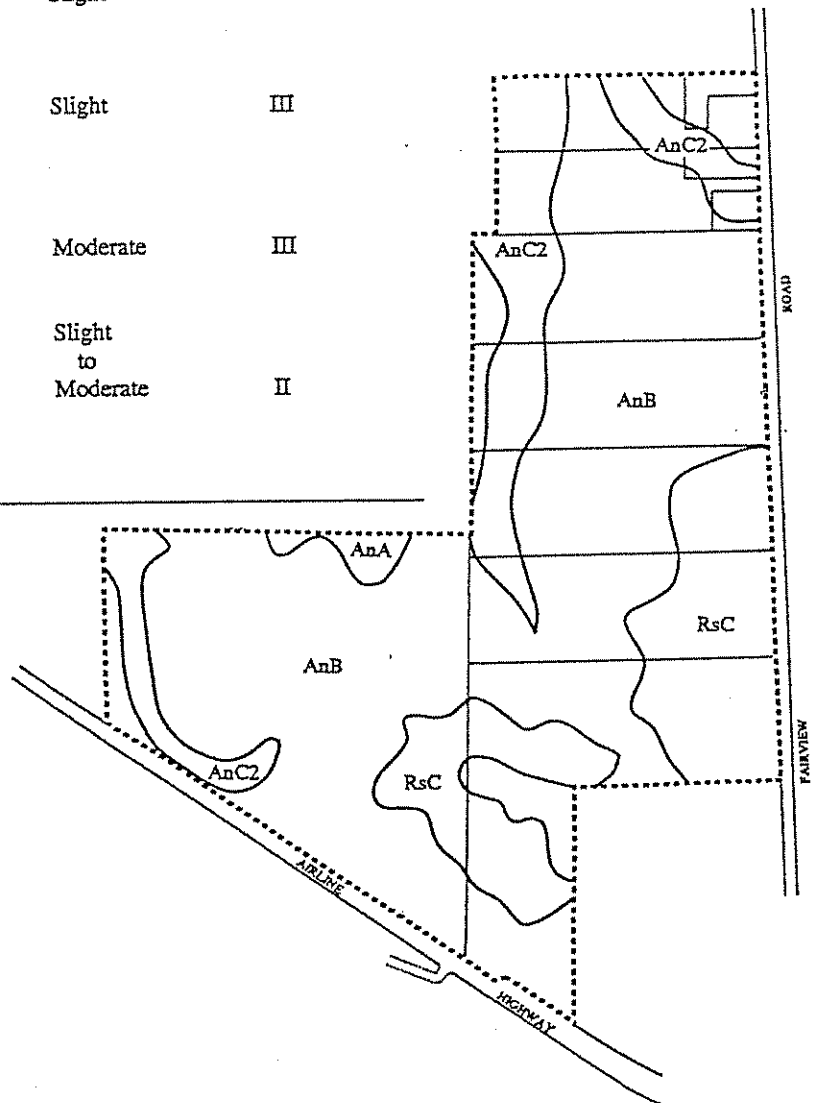
FIGURE 5.2

Site Soils

TABLE 5.1 - SITE SOILS

SOIL CLASSIFICATION	RUN-OFF	EROSION	AGRICULTURAL CAPABILITY
AnA - Antioch Loam, 0 - 2% Slope	Very Slow	Slight	III
AnB - Antioch Loam, 2 - 5% Slope	Slow	Slight	III
AnC2 - Antioch Loam, 5 - 9% Slope, Eroded	Slight to Medium	Moderate	III
RsC - Rincon Silty Clay Loam, 2 - 9% Slope	Slow to Medium	Slight to Moderate	II

Source: San Benito County Soil Survey



5.5 ENERGY CONSERVATION

The accommodation of public transit and bicycle and pedestrian traffic within the Specific Plan area will contribute to the reduction of the amount of energy consumed by Specific Plan area residents. (See Section 4.0.) In addition, energy conservation measures required by current energy codes will reduce air heating and cooling, as well as water heating, requirements. The energy conservation policies presented in this Specific Plan augment the statutory requirements. Policies and supporting diagrams included in Section 6.0, Community Design, expand upon and illustrate the conservation of energy through solar techniques.

5.6 FLORA AND FAUNA

Historically, the Specific Plan area has been used for limited agricultural and grazing activities. Given this use, native vegetation within the Plan vicinity has not been able to reside on the project site. To encourage the conservation of native vegetation within the Plan area, native plant species will be used to the greatest extent feasible for parkway and residential landscaping.

5.7 AIR QUALITY

As previously discussed, the Specific Plan land use and circulation plans include the provision of an extensive bikeway and pedestrian pathway system. In addition, public transit rider facilities will be provided throughout the Plan area, as specified by SANTRANS and City Public Works directors. Such measures promote the conservation of regional air quality.

In addition, the West Fairview Road Specific Plan EIR contains a thorough discussion of the Project's relationship to the regional Air Quality Management Plan. Please refer to that document for further information on air quality.

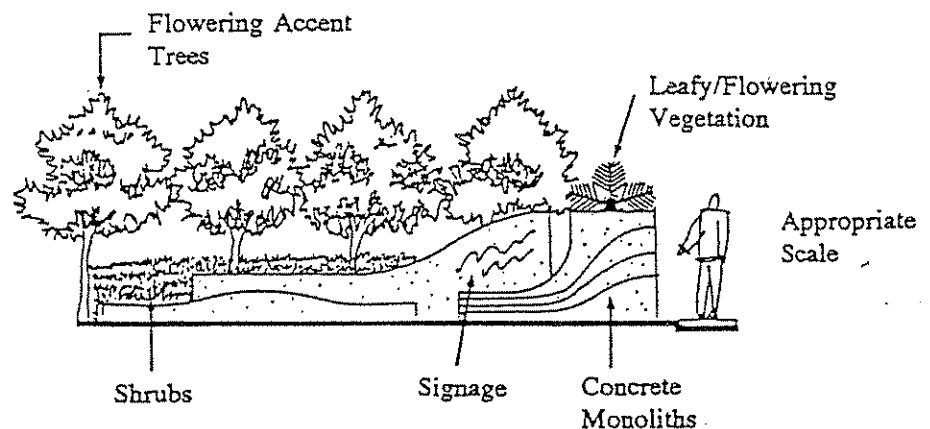
Policy 6.4.2 Major intersections and crosswalks shall be defined through special pavement treatment such as precast hexagonal pavers, stamped concrete, or other suitable decorative surface.

Policy 6.4.3 Entry points into the West Fairview Specific Plan area and individual residential neighborhoods may be marked by identification signs. In general, signs should have low profile and relate to their surroundings in terms of size, shape, color, texture and lighting. (See Figure 6.9.)

Action: a. The use of natural materials such as wood, brick or stone for signage shall be encouraged, although metal lettering is permitted.

FIGURE 6.9

Entry Way Signage



Policy 6.4.4 Landscaped medians shall be provided at project entrances and arterial intersections to enhance their aesthetic appeal.

Objective 5: Maintenance of acceptable noise levels within exterior and interior residential environments.

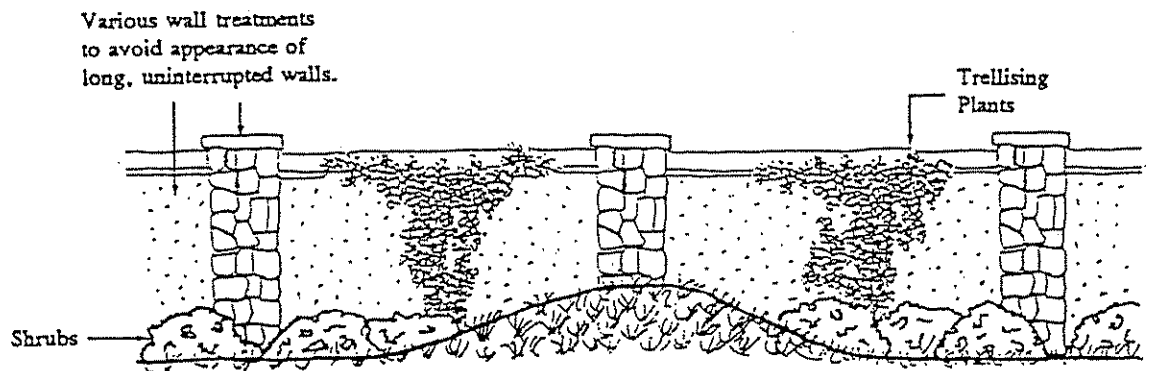
Policy 6.5.1 Noise attenuation devices shall be designed and constructed to ensure that exterior noise exposures within the Project area will not exceed 60 Db DNL.

- Action: a. Exterior noise attenuation devices shall be constructed in accordance with the standards set for in Appendix A, Design Standards.

Policy 6.5.2 Noise attenuation devices, which incorporate aesthetic design features, shall be located within the landscape buffers along Airline Highway and Fairview Road. (See Figure 6.10.)

FIGURE 6.10

Aesthetic Design Features for Sound Walls



- Actions:
- Landscaping, mounding, and berms shall be used in conjunction with walls as a way of mitigating noise impacts from Airline Highway and Fairview Road (as opposed to free-standing walls).
 - A continuity of landscape and wall treatments along Airline Highway and Fairview Road shall be encouraged.
 - Sound barrier designs shall incorporate regular articulations; the use of long, uninterrupted walls for privacy and/or sound mitigation shall be discouraged.
 - To soften the face of walls, planters which allow cascading materials and/or trellising plant species shall be encouraged.
 - Treatment of noise barriers shall be established and approved as part of the design review process.

Policy 6.5.3 To protect future residences from noise generated along Airline Highway and Fairview Road, two-story homes shall be discouraged along these frontages.

Policy 6.5.4 Residential structures near Airline Highway and Fairview Road shall be designed and constructed to ensure that interior noise levels do not exceed 45 Db DNL.

Action: a. Interior noise control measures, as described in Appendix A, Design Standards, shall be incorporated into dwelling units designs where necessary.

Objective 6: Project area lighting that adequately provides for the safety of residents and motorists and avoids creating excessive glare.

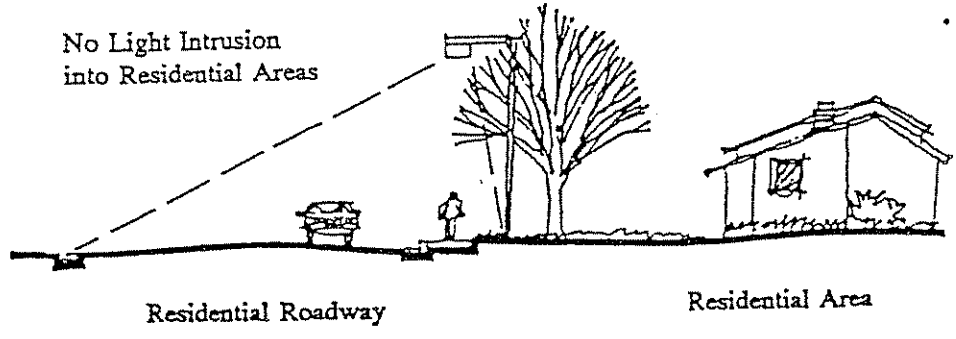
Policy 6.6.1 Exterior lighting shall be provided for safety and security, as well as enhancing building design and landscaping.

Action: a. Lighting fixture design shall be compatible with building design.

Policy 6.6.2 Exterior lighting shall be designed to project light downward only, so as not to create glare for project occupants or neighboring properties. (See Figure 6.11.)

FIGURE 6.11

Projection of Outdoor Lighting



Policy 6.6.3 All street lighting shall be designed to utilize energy efficient illumination devices and shall be of the minimum intensity necessary to provide for the safety needs of motorists and pedestrians. The siting of street lighting shall be limited to only those locations warranted.

- Actions:
- a. The design of all project street lighting shall be subject to the review and approval of the City Engineering Director and Planning Director.
 - b. Street lighting shall be designed and constructed in accordance with the City's Standard Specifications and Details.

Policy 6.2.1: All dwelling units and fences constructed in the project area shall be of quality design and shall utilize quality materials.

- Actions:
- a. Materials, textures, colors and details of all structures should be an appropriate expression of the development's design concept and function.
 - b. Diversity in building design shall be accomplished through provision of a variety of building plans and elevations to avoid the appearance of a monotonous "tract."

Single family dwelling units shall have at least (3) of the following five (5) design elements different from single family dwelling units on any adjacent lot on the same street frontage:

- (a) Floor plan
 - (b) Front elevation
 - (c) Roof design
 - (d) Exterior materials
 - (e) Reverse floor plan
- c. Multiple family dwelling units shall maintain single family character through use of appropriate building massing and materials.
 - d. All exterior wall elevations visible from and/or facing streets are to have architectural treatment. No building surface on a street shall have a flat, void surface without architectural treatment or elements. Examples of architectural treatment include exterior finishes, siding, and stucco and paint patterns. Examples of architectural elements includes facade modulation and articulation, windows, canopies and balconies.
 - e. A variety of complementary architectural materials shall be used to create a pleasant streetscape. Such materials may include wood shingles, horizontal and vertical wood siding, stucco, brick and stone. (See Figure 6.2.)
 - f. A variety of roof designs shall be utilized. Gable, hip, dormers and shed type roofs are encouraged. Roof materials shall be Class A or B fire rated and non-reflective.

FIGURE 6.2

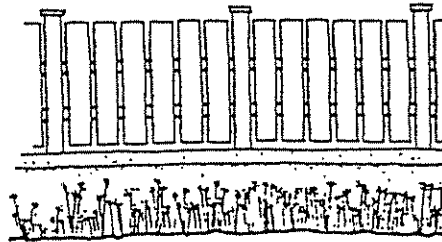
*Architectural Variation
through Design Features*



- g. Single family dwellings should incorporate a variety of garage types and locations. This may be achieved through provision of some carriage-entry garages and garages setback from the street in addition to garages facing the street. Variety in garage size (2- and 3-car) and garage door treatment is also encouraged.
- h. All building projections including chimneys, flues, and vents shall be compatible in height and material with the structure from which they project.
- i. All fences and walls should reflect the architectural character of dwelling units, incorporating similar colors, materials and detailing.
- j. Use of open fencing and landscaping for privacy barriers, rather than use of solid walls or fences, is encouraged, except where walls or fences are used as sound barriers. (See Figure 6.3.)
- k. A maximum fence height of six feet should be maintained, except where fences are used as sound barriers adjacent to Airline Highway.

FIGURE 6.3

Open Fencing



1. All project designs will be subject to design review and approval by the City.

Policy 6.2.2 Encourage creative solutions which successfully integrate below market rate and market rate housing units.

- Actions:
- a. Architectural treatment and detail for multi-family and senior projects should create visual interest through orientation, form and alignment of individual buildings.
 - b. Architectural treatment and detail for duette units should emulate single family dwellings to the greatest extent feasible.
 - c. Architectural treatment and materials used for the front elevations of affordable housing units shall be visually compatible with market rate units.
 - d. All project floor plans and elevations will be subject to design review and approval by the City.

Policy 6.2.3 Site design and architecture shall consider solar access, wind protection, and shade, depending upon the time of year, to enhance the quality of outdoor space. (See Figure 6.4.)

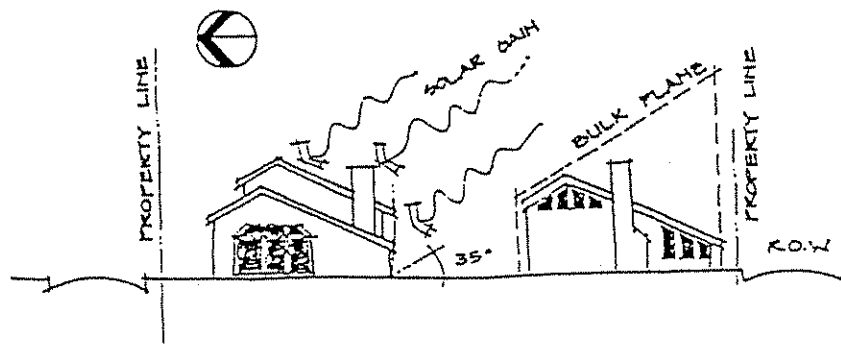
- Action:
- a. Buildings should be designed to minimize mechanical heating and cooling. Sunlight should be used for direct heating and illumination wherever possible. Shading should be used to cool a building. Active and passive solar heating is encouraged.

- b. The planting of deciduous trees in locations to aid summer cooling and allow solar gain for winter heating, shall be encouraged.

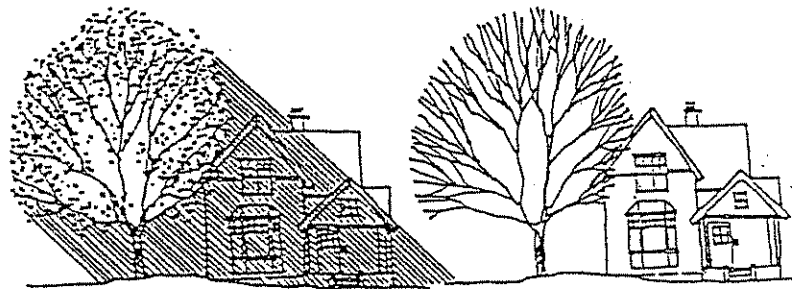
FIGURE 6.4

Solar Orientation in Site and Architectural Design

Careful attention needs to be given to the land use plan regarding bulk, so that the adjacent unit is not shadowed. In addition, the roof and south facing wall can be utilized for passive or active solar techniques.



Deciduous trees correspond to summer/winter angles by blocking out unwanted summer sun, while allowing winter sun to shine through; once leaves have dropped.



Policy 6.2.4 Minimize the apparent bulk of buildings, especially from Airline Highway and Fairview Road, and recreational/open space areas.

- Actions:
- a. A landscape buffer shall be maintained along Airline Highway and Fairview Road.
 - b. Landscaping within the neighborhood park and parkway areas shall be designed to help screen views of neighboring residential structures.
 - c. The siting of one-story buildings or building elements facing Airline Highway and Fairview Road is encouraged.

SECTION 6.0

COMMUNITY DESIGN

6.1 COMMUNITY DESIGN CONCEPT

The concept for the development of the residential community within the West Fairview Specific Plan area focuses on the compatibility of future project neighborhoods with established Hollister neighborhoods. In addition, the provision of a variety of housing types that meet the needs of various income levels is a primary goal of this Specific Plan. Finally, the availability and accessibility of recreational/open space areas to project residents is featured. In general, the Specific Plan places emphasis on the integration of the development with the existing urban and rural environment.

The West Fairview Specific Plan area is comprised of nine parcels (Planning Area A). To discourage the appearance of a series of unrelated or independent subdivisions, this element was prepared to ensure visual coherence and compatibility throughout the Plan area.

6.2 COMMUNITY DESIGN POLICIES

The following community design policies expand upon the goals, objectives, policies and implementation actions presented in each preceding section and offer a development methodology for the West Fairview Specific Plan area. The community design policies address architectural and landscape design, as well as the treatment of entrances, major intersections, signage, sound attenuation and lighting. Development standards defining lot sizes, setbacks, site coverage, height limitations and parking requirements follow the community design policies.

Goal: High quality residential design and construction in the West Fairview Road Specific Plan area that complements the identity of the City of Hollister as a whole.

Objective 1: To create a coherent land use pattern, unified by a larger organizational framework of streets and entries, and recreational/open space areas.

Policy 6.1.1 A system of open space easements which provide visual and physical access to recreational/open space areas shall be developed. (See Figure 6.1.)

- Action: a. As illustrated in the Land Use Plan, Figure 2.1, approximately 7-8 acres of land bordering Crestview Drive and the main project arterial shall be designated for a linear parkway. The parkway will terminate to the south at an approximately 5 acre neighborhood park.

Policy 6.1.2 Higher densities shall be encouraged to locate adjacent to the recreational/open space system and major roadways.

- Actions: a. Approximately 7.0 acres located south of the intersection of Crestview Drive and the project arterial shall be designated for Medium Density residential use with a maximum allowable density of eight units per acre (gross).
- b. The development standards for Small Single Family Lots, Zero Lot Lines, Duettes, Senior and Multiple Family housing, as set forth in Section 6.3 of this Plan, are particularly well suited to those areas designated for Medium Density Residential in Figure 2.1, Land Use Plan.
- c. Parking areas for Medium density housing should be screened from public streets with landscaping, walls, fences, berms or other means. When warranted by size or visibility, islands of trees shall be incorporated into parking areas.

Policy 6.1.3 Lower density residential uses shall be located adjacent to, and provide a buffer for, existing off-site half-acre and one-acre developments.

- Actions: a. Areas located within approximately 150 feet and contiguous to existing half-acre and one-acre developments (north and south of the Plan area) shall be designated for large single family lots (minimum 20,000 square feet).
- b. The development standards for large single family lots set forth in Section 6.3 of this Plan shall apply to those areas so designated in Figure 2.1, Land Use Plan.

Policy 6.1.4 The balance of the Specific Plan area designated for residential uses should be developed with a mix of housing types and densities.

- Action: a. In all residential areas not designated for large lots, medium density, or parkland in Figure 2.1, Land Use Plan, residential uses may include any of the housing types and

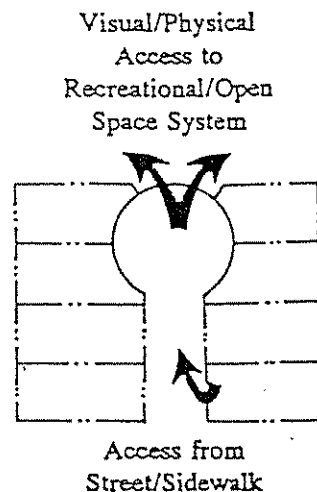
densities identified in Section 6.3 of this Plan, as long as the average density does not exceed 5.4 units per gross acre project-wide.

Policy 6.1.5 Internal circulation shall be organized in a logical pattern to unify and strengthen neighborhood character.

- Actions:
- a. Local streets within Specific Plan neighborhoods shall be designed to reflect a modified-grid pattern.
 - b. Use of cul-de-sacs shall generally be discouraged, except in cases where the cul-de-sac serves as a viewshed to recreational/open space parklands, or irregular lot configuration warrants cul-de-sac design.
 - c. Where cul-de-sacs terminate at open space areas, open space easements shall be provided (see Figure 6.1)

FIGURE 6.1

*Open Space Easements to
Recreational/Open Space Areas*



Objective 2: To create a coherent development pattern composed of buildings with complementary architectural character and quality design, while providing visual interest and variation.

Policy 6.2.1: All dwelling units and fences constructed in the project area shall be of quality design and shall utilize quality materials.

- Actions:
- a. Materials, textures, colors and details of all structures should be an appropriate expression of the development's design concept and function.
 - b. Diversity in building design shall be accomplished through provision of a variety of building plans and elevations to avoid the appearance of a monotonous "tract."

Single family dwelling units shall have at least (3) of the following five (5) design elements different from single family dwelling units on any adjacent lot on the same street frontage:

- (a) Floor plan
 - (b) Front elevation
 - (c) Roof design
 - (d) Exterior materials
 - (e) Reverse floor plan
- c. Multiple family dwelling units shall maintain single family character through use of appropriate building massing and materials.
 - d. All exterior wall elevations visible from and/or facing streets are to have architectural treatment. No building surface on a street shall have a flat, void surface without architectural treatment or elements. Examples of architectural treatment include exterior finishes, siding, and stucco and paint patterns. Examples of architectural elements includes facade modulation and articulation, windows, canopies and balconies.
 - e. A variety of complementary architectural materials shall be used to create a pleasant streetscape. Such materials may include wood shingles, horizontal and vertical wood siding, stucco, brick and stone. (See Figure 6.2.)
 - f. A variety of roof designs shall be utilized. Gable, hip, dormers and shed type roofs are encouraged. Roof materials shall be Class A or B fire rated and non-reflective.

FIGURE 6.2

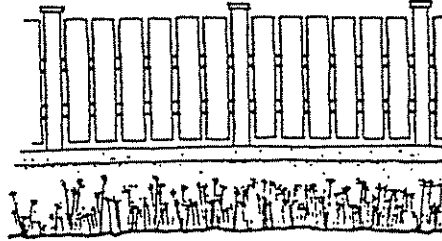
*Architectural Variation
through Design Features*



- g. Single family dwellings should incorporate a variety of garage types and locations. This may be achieved through provision of some carriage-entry garages and garages setback from the street in addition to garages facing the street. Variety in garage size (2- and 3-car) and garage door treatment is also encouraged.
- h. All building projections including chimneys, flues, and vents shall be compatible in height and material with the structure from which they project.
- i. All fences and walls should reflect the architectural character of dwelling units, incorporating similar colors, materials and detailing.
- j. Use of open fencing and landscaping for privacy barriers, rather than use of solid walls or fences, is encouraged, except where walls or fences are used as sound barriers. (See Figure 6.3.)
- k. A maximum fence height of six feet should be maintained, except where fences are used as sound barriers adjacent to Airline Highway.

FIGURE 6.3

Open Fencing



1. All project designs will be subject to design review and approval by the City.

Policy 6.2.2 Encourage creative solutions which successfully integrate below market rate and market rate housing units.

- Actions:
- a. Architectural treatment and detail for multi-family and senior projects should create visual interest through orientation, form and alignment of individual buildings.
 - b. Architectural treatment and detail for duette units should emulate single family dwellings to the greatest extent feasible.
 - c. Architectural treatment and materials used for the front elevations of affordable housing units shall be visually compatible with market rate units.
 - d. All project floor plans and elevations will be subject to design review and approval by the City.

Policy 6.2.3 Site design and architecture shall consider solar access, wind protection, and shade, depending upon the time of year, to enhance the quality of outdoor space. (See Figure 6.4.)

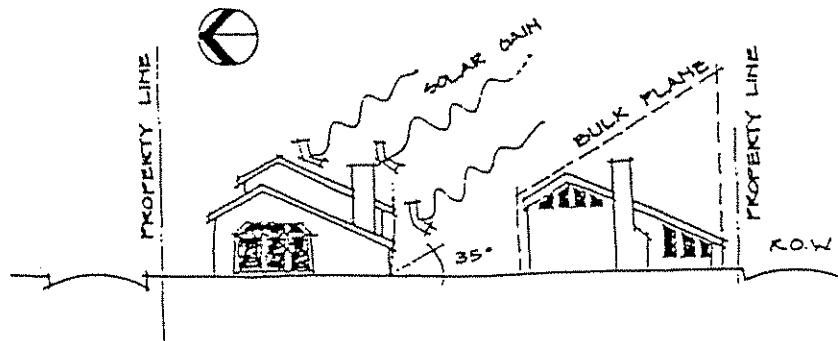
- Action:
- a. Buildings should be designed to minimize mechanical heating and cooling. Sunlight should be used for direct heating and illumination wherever possible. Shading should be used to cool a building. Active and passive solar heating is encouraged.

- b. The planting of deciduous trees in locations to aid summer cooling and allow solar gain for winter heating, shall be encouraged.

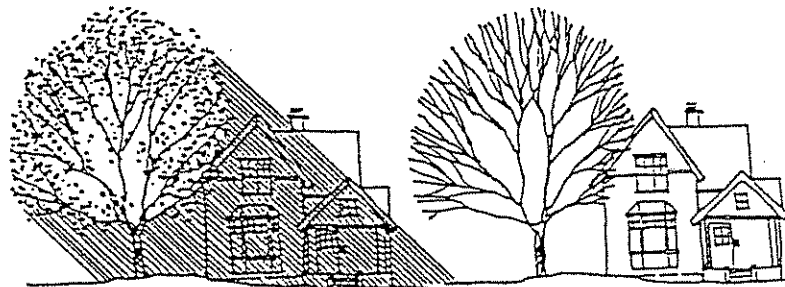
FIGURE 6.4

Solar Orientation in Site and Architectural Design

Careful attention needs to be given to the land use plan regarding bulk, so that the adjacent unit is not shadowed. In addition, the roof and south facing wall can be utilized for passive or active solar techniques.



Deciduous trees correspond to summer/winter angles by blocking out unwanted summer sun, while allowing winter sun to shine through; once leaves have dropped.



Policy 6.2.4 Minimize the apparent bulk of buildings, especially from Airline Highway and Fairview Road, and recreational/open space areas.

- Actions:
- a. A landscape buffer shall be maintained along Airline Highway and Fairview Road.
 - b. Landscaping within the neighborhood park and parkway areas shall be designed to help screen views of neighboring residential structures.
 - c. The siting of one-story buildings or building elements facing Airline Highway and Fairview Road is encouraged.

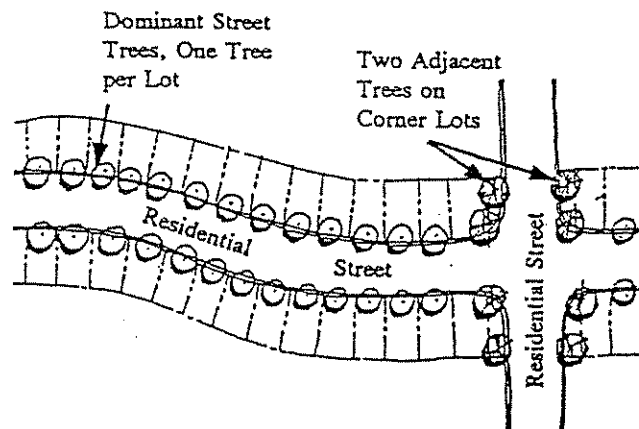
Objective 3: Landscaping that complements site and building design and enhances the park landscape where appropriate.

Policy 6.3.1 Street trees shall be planted throughout the project area.

- Actions:
- a. To serve as a unifying element, street trees shall be provided along project arterial and collector roadways, and linear parkways. The orientation of street trees should visually define the public use areas and separate roadways from pedestrian spaces.
 - b. On residential streets, a minimum of one tree per lot (two trees per corner lot) shall be planted by the project developers. Trees shall be planted 5 to 8 feet from the back of the sidewalk, so that a canopy of street shade is provided. (See Figure 6.5.)

FIGURE 6.5

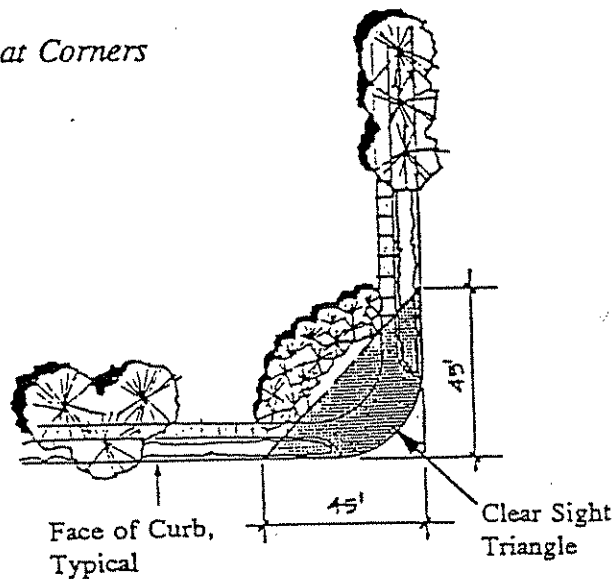
Residential Street Tree Planting



- c. All trees planted adjacent to streets shall be a minimum fifteen gallon size; all others shall be a minimum five gallon size.
- d. Landscaping shall be designed to maintain appropriate sight line clearances at corners. (See Figure 6.6)

FIGURE 6.6

Sight Line Clearances at Corners



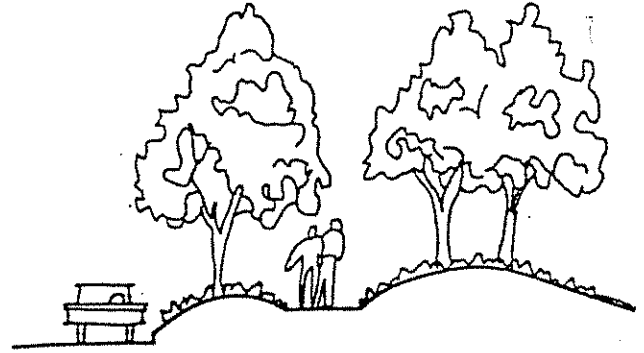
Policy 6.3.2 Neighborhood park and linear park landscaping shall be designed to provide aesthetically pleasing open space areas that complement surrounding residential areas.

- Actions:
- The developer(s) shall prepare a typical landscape plan for the neighborhood park and linear park areas which address plant materials, the provision of pedestrian amenities such as street furniture, street lighting, and paving materials in sidewalk and crossing areas.
 - Parkland areas shall be constructed and landscaped during the first phase of individual developments, including park facilities, walls, walkways, signage and lighting.
 - Landscaping which utilizes mounding and berming as a means of adding visual interest should be encouraged in parkland areas. (See Figure 6.7.)

FIGURE 6.7

Landscape Mounding and Berming

Creating visual interest by providing mounding in parkland, entry way and landscape buffer areas.



Policy 6.3.3 Landscaping of common areas for multiple family dwellings shall be provided and maintained at the time dwelling units are occupied. All unpaved areas shall be landscaped with grass, groundcover and/or shrub plant material.

Policy 6.3.4 Streetscape landscaping shall be designed in accordance with the guidelines and standards contained in the City of Hollister Streetscape Improvement Plans (see Appendix A, Design Standards).

- Actions:
- a. Landscape designs for neighborhood park, linear park and street landscaping will be subject to design review and approval by the City Engineering Director and Parks Director.
 - b. All plants shall be planted with spacings and locations (given the plant types and character, type of soil, availability of or likelihood of watering regularity and similar considerations) so that the plantings will achieve their purpose within a reasonable time.

Objective 4: Aesthetically pleasing project entrances, major intersections and signage.

Policy 6.4.1 Major entry ways into the West Fairview Road Specific Plan area and project neighborhoods shall be enhanced through the use of landscape amenities such as earth mounding and berming, medians, pavement treatment, entry walls and signage. (See Figure 6.8. for an example entry concept)

- Action:
- a. Project and neighborhood entrances shall be constructed and landscaped during the first phase of individual developments, including walls, walkways, signage, and lighting.

6.3 DEVELOPMENT STANDARDS

The following are the minimum development standards required for development within the Plan area. As described previously in this Section, residential areas may contain a variety of housing types and densities, with the exception of lands designated for large lots. However, an overall maximum density for the project of 5.4 units per gross acre may not be exceeded.

Large single family lot of a minimum 20,000 square feet (see Figure 6.12.):

Setbacks*:
Front yard - 18 feet minimum**, 20' feet average
Rear yard - 20 feet minimum
Side yard - 10 feet minimum.

Site Coverage: 40 percent maximum

Height Limitations: 30 feet maximum

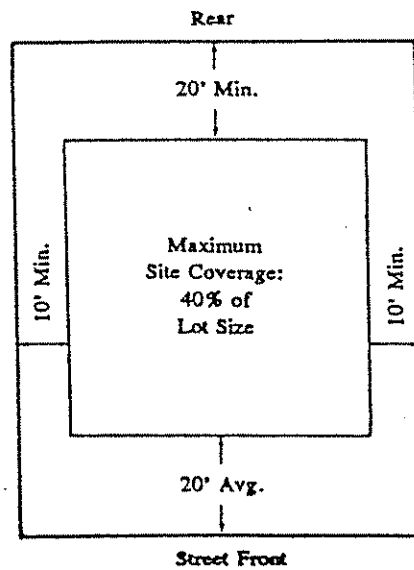
Parking Requirements: 2 car garage minimum
3 car garage maximum

* For corner lots, the sideyard with roadway frontage shall have a minimum setback of 15 feet.

** Articulated garage doors required.

FIGURE 6.12

Large Single-Family Lot



Standard single family lot of a minimum 6,000 square feet (see Figure 6.13.):

Setbacks*: Front yard - 18 feet minimum**, 20' feet average
Rear yard - 15 feet minimum
Side yard - 6 feet minimum or 10% of lot width; but in no case less than 4 feet.

Site Coverage: 40 percent maximum

Height Limitations: 30 feet maximum

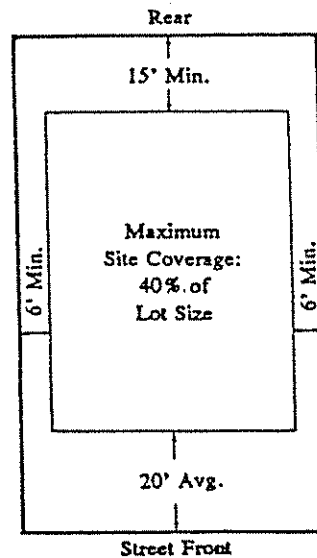
Parking Requirements: 2 car garage

* For corner lots, the sideyard with roadway frontage shall have a minimum setback of 10 feet.

** Articulated garage doors required.

FIGURE 6.13

Standard Single-Family Lot



Standard single family lot of a minimum 6,000 square feet (see Figure 6.13.):

Setbacks*: Front yard - 18 feet minimum**, 20' feet average
Rear yard - 15 feet minimum
Side yard - 6 feet minimum or 10% of lot width; but in no case less than 4 feet.

Site Coverage: 40 percent maximum

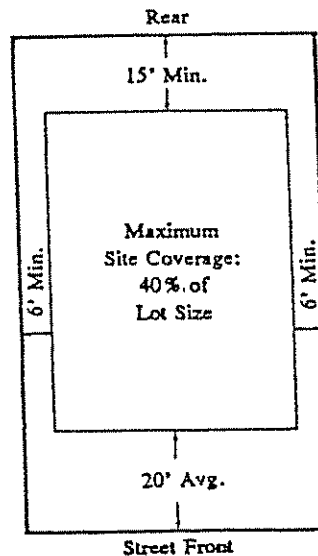
Height Limitations: 30 feet maximum

Parking Requirements: 2 car garage

- * For corner lots, the sideyard with roadway frontage shall have a minimum setback of 10 feet.
- ** Articulated garage doors required.

FIGURE 6.13

Standard Single-Family Lot



Small single family lot of a minimum 4,000 square feet (see Figure 6.14.):

Setbacks*: Front yard - 15 feet minimum to structure
20 feet minimum to garage
Rear yard - 15 feet minimum
Side yard - 4 feet minimum

Site Coverage: 50 percent maximum

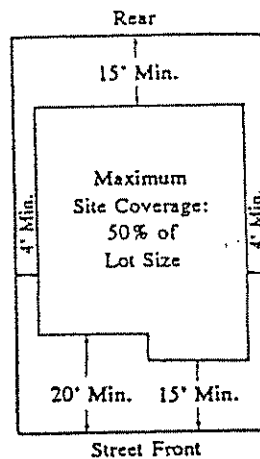
Height Limitations: 30 feet maximum

Parking Requirements: 1 car garage
1 uncovered space

* For corner lots, the sideyard with roadway frontage shall have a minimum setback of 10 feet.

FIGURE 6.14

Small Single-Family Lot



Zero lot line single family lot of a minimum 4,000 square feet (see Figure 6.15.):

Setbacks*: Front yard - 15 feet minimum to structure
20 feet minimum to garage
Rear yard - 15 feet minimum
Side yard - 8 feet minimum

Site Coverage: 50 percent maximum

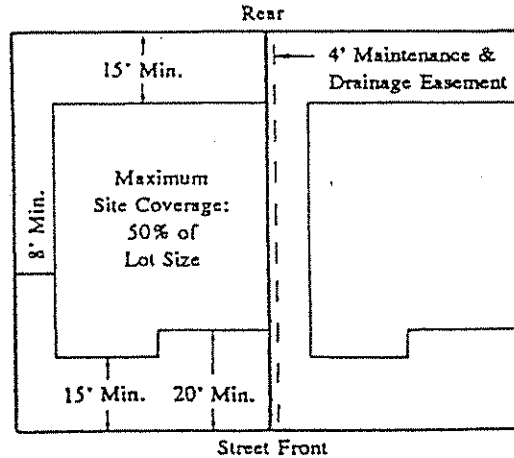
Height Limitations: 30 feet maximum

Parking Requirements: 1 car garage
1 uncovered space

For corner lots, the sideyard with roadway frontage shall have a minimum setback of 10 feet.

FIGURE 6.15

Zero Lot Line Single-Family Lot



Zero lot line single family lot of a minimum 4,000 square feet (see Figure 6.15.):

Setbacks*: Front yard - 15 feet minimum to structure
20 feet minimum to garage
Rear yard - 15 feet minimum
Side yard - 8 feet minimum

Site Coverage: 50 percent maximum

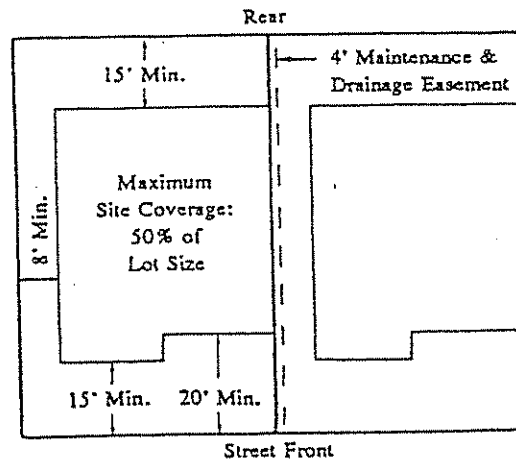
Height Limitations: 30 feet maximum

Parking Requirements: 1 car garage
1 uncovered space

For corner lots, the sideyard with roadway frontage shall have a minimum setback of 10 feet.

FIGURE 6.15

Zero Lot Line Single-Family Lot



Duette lot of a minimum 7,000 square feet (see Figure 6.16.):

Setbacks*: Front yard - 15 feet minimum to structure
20 feet minimum to garage
Rear yard - 15 feet minimum
Side yard - 6 feet minimum

Site Coverage: 25 percent maximum per dwelling unit

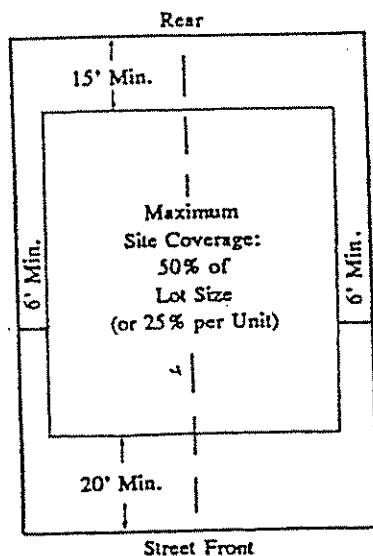
Height Limitations: 30 feet maximum

Parking Requirements: 1 car garage per dwelling unit
1 uncovered space per dwelling unit

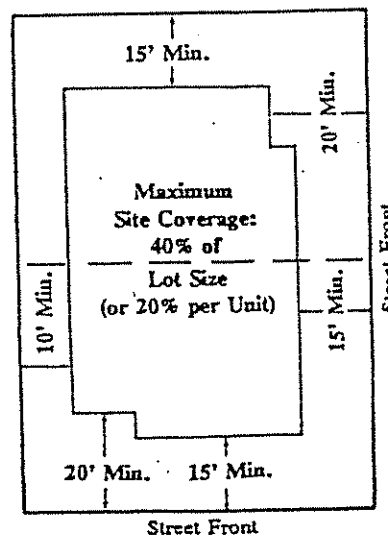
For corner lots, the sideyard with roadway frontage shall have a minimum setback of 10 feet.

FIGURE 6.16

Duette Lot



STANDARD LOT



CORNER LOT

Senior and Multiple Family Housing

Development of senior and multi-family housing shall adhere to the guidelines set forth by the City of Hollister Zoning Ordinance, Section 24: Low Density Multiple Residence District, R3 District. Standards set forth in that Ordinance include the following:

- Setbacks:**
- Front yard - 15 feet minimum to structure
20 feet minimum to garage
 - Rear yard - 15 feet minimum *
 - Side yard - 6 feet or 10 percent of the lot width if that be less; but in no case shall it be less than 4 feet; provided that the side yard on the street side of a corner lot which is adjacent to a key lot shall have a width not less than half the depth of the front yard required on such key lot up to a maximum requirement of 10 feet for such side yard.*
- Site Coverage:** 50 percent maximum
- Height Limitations:** 30 feet maximum
- Parking Requirements:** 2 spaces per dwelling unit for the first 4 units and 1 - 1/2 space for each additional unit

* Except as otherwise specified for dwelling groups as defined by the City of Hollister Zoning Ordinance, Section 24.8 - Low Density Multiple Residence District, R-3 District.

Senior and Multiple Family Housing

Development of senior and multi-family housing shall adhere to the guidelines set forth by the City of Hollister Zoning Ordinance, Section 24: Low Density Multiple Residence District, R3 District. Standards set forth in that Ordinance include the following:

- Setbacks:**
- Front yard - 15 feet minimum to structure
20 feet minimum to garage
 - Rear yard - 15 feet minimum *
 - Side yard - 6 feet or 10 percent of the lot width if that be less; but in no case shall it be less than 4 feet; provided that the side yard on the street side of a corner lot which is adjacent to a key lot shall have a width not less than half the depth of the front yard required on such key lot up to a maximum requirement of 10 feet for such side yard.*
- Site Coverage:** 50 percent maximum
- Height Limitations:** 30 feet maximum
- Parking Requirements:** 2 spaces per dwelling unit for the first 4 units and 1 - 1/2 space for each additional unit

* Except as otherwise specified for dwelling groups as defined by the City of Hollister Zoning Ordinance, Section 24.8 - Low Density Multiple Residence District, R-3 District.

SECTION 7.0

PUBLIC SERVICES AND UTILITIES

7.1 PUBLIC SERVICE AND UTILITY POLICIES

As required by the State of California Office of Planning and Research, goals, objectives, policies and implementation actions pertaining to public services and utilities are contained below. These statements address infrastructure improvements including sanitary sewer, water and storm drainage. In addition, police and fire protection, schools, solid waste disposal and utilities are addressed. All policy statements contained in this Specific Plan are based on, and are consistent with, the goals and policies defined in the City of Hollister General Plan.

The infrastructure improvements set forth in this Plan are limited to the major infrastructure elements needed for anticipated future development. More detailed infrastructure planning for specific development proposals will need to be undertaken at the time these projects are proposed.

The following goals, objectives, policies and implementation measures are expanded upon by a subsequent narrative description of proposed transportation improvements and supporting illustrations.

Goal: Adequate provision of services and an efficient system of public utilities.

Objective 1: To ensure that adequate infrastructure facilities for water, sewer and storm drainage are incorporated into the project.

Policy 7.1.1: Water lines shall be designed and constructed to adequately provide for water service and fire protection needs of all new planned development.

- Actions:**
- a. New water lines shall be sized to provide for adequate fire flow. Water line sizes are illustrated in Figure 7.1.
 - b. Utilize water conservation strategies in the design of all new water facilities.

Policy 7.1.2: Separate water meters shall be provided for each residential unit.

Policy 7.1.3: Sewer lines shall be designed and constructed to adequately serve new development.

- Actions:
- a. Sewer facilities shall be designed to assure sufficient capacity to handle anticipated flows (Figure 7.2). Gravity flow shall be provided wherever possible. Sewage pump stations and force mains shall be provided where required.
 - b. Sewer facilities shall be designed and constructed in accordance with the City's Public Works Master Plan.
 - c. Construction documents for all sewer facilities shall be subject to the review and approval of the City Engineering Director.

Policy 7.1.4: Storm drainage systems shall be designed to maximize groundwater recharge where feasible.

- Actions:
- a. On-site stormwater detention ponds shall be provided to mitigate loss of recharge areas where feasible.
 - b. Storm drains shall be constructed to transmit stormwater to detention/retention basins and to final discharge points, as illustrated in Figure 7.3.

Policy 7.1.5: All storm drainage facilities shall conform to the City of Hollister Standard Specifications and Details.

Policy 7.1.6: Sediment traps, evaporation basins, and flow reduction devices and other methods to reduce the volume of grease and oil pollutants in street surface runoff shall be installed in the storm drain system as directed by the City Engineering Director.

Policy 7.1.7: Proponents of the Specific Plan shall be responsible for construction of major water infrastructure which will be necessary and will not be constructed as a part of individual project applications. In order for development to occur on a project-by-project basis, major water infrastructure must be designed to allow access to individual project areas without necessitating construction of the entire water infrastructure system. Designs for these improvements shall be submitted to the City Public Works Director for review and approval prior to issuance of a grading permit, encroachment permit, building permit, or approval of any tentative subdivision map for construction of the major infrastructure.

Policy 7.1.8: The individual project proponents shall submit site improvement plans to officials at the Sunnyslope County Water District and to the City Engineering Director for evaluation of the necessity of additional water facilities (wells, pressure pumps, storage tanks, etc.) at the Specific Plan site, for review and approval prior to approval of any tentative subdivision map. In addition, any improvements to the water system at the proposed project site determined to be necessary in The Sunnyslope Ground-water Resources Report (Todd Engineers, August 1993) shall be included in the site improvement plans, for review and approval prior to approval of any tentative subdivision map. Water facilities provided by individual project proponents that will benefit other individual projects are subject to reimbursement agreements. Agreements to provide additional water facilities shall be in place prior to issuance of a grading permit, encroachment permit, building permit, or approval of any tentative subdivision maps for individual projects, subject to review and approval of the City Attorney.

Policy 7.1.9: Individual project proponents shall be responsible for obtaining a guarantee of service from Sunnyslope County Water District (SCWD) and/or some other water purveyor, subject to review and approval of the City of Hollister Engineering Director prior to issuance of any building permits for the proposed project.

Goal: Adequate police and fire protection for all Specific Plan residents.

Objective 2: To supplement existing police and fire abilities to provide police and fire protection services for all Specific Plan residents.

Policy 7.2.1: Police Department recommendations regarding design aspects of development that affect traffic safety and crime prevention shall be incorporated into all new development.

Action: a. Final designs for all projects in the Specific Plan area shall be subject to the review of the City Police Chief.

Policy 7.2.2: All development within the Specific Plan area shall conform to applicable fire codes and development standards.

Action: a. Final designs for all projects in the Specific Plan area shall be subject to the review of the City Fire Chief.

Policy 7.2.3: Police and fire impact fees shall be dedicated for each individual project development phase.

- Action:
- a. Police and fire impact fees shall be paid by the developer at the time building permits are issued.
 - b. Project proponents shall establish, through a community facilities district (Mello-Roos), funding for fire and police services. These funds along with items "a" and "c" under this policy will fully mitigate project police and fire impacts.
 - c. Project proponents shall negotiate an agreement with the Hollister Fire Department to reserve approximately 8,000 square feet of land for a fire sub-station.

Policy 7.2.4: In addition to fire impact fees and participation in a Mello-Roos Community District, project proponents shall reserve 8,000 square-feet of land for a fire sub-station. Reservation of land shall be made via an agreement similar to the one developed with the school district. The location of the fire sub-station shall be mutually agreed upon between the project proponents and the Fire Chief.

Goal: Quality educational opportunities for all school-age residents in the Specific Plan area.

Objective 3: To supplement existing school facilities, staffing, and equipment to support an adequate elementary and high school education for all school-age residents within the Specific Plan area.

Policy 7.3.1: School impact fees shall be dedicated for each individual project development phase.

- Action:
- a. School impact fees shall be paid by the developer at the time building permits are granted.
 - b. The project proponents shall enter into an agreement with the local school districts to provide for additional school impact fees. These are to be paid at the time building permits are issued or residential units are sold.
 - c. The project proponents shall negotiate an agreement with the Hollister Elementary School District to reserve up to 10 acres of land for an elementary school site.

Goal: Collection and disposal of generated wastes in an environmentally safe manner.

Objective 4: To dispose of solid waste associated with new development in a manner which will protect the environment and ensure public health and safety.

Policy 7.4.1: Solid wastes generated by development in the West Fairview area shall be handled and disposed of in accordance with the City and County Integrated Solid Waste Management Plan.

Policy 7.4.2: Cooperate with the City in promoting recycling of solid waste material.

Goal: Utility services which are sufficient to meet anticipated demands for the West Fairview area.

Objective 5: To ensure that the design and construction of utilities are both energy efficient and aesthetic in appearance.

Policy 7.5.1: The planning and scheduling of future utilities which will serve the Specific Plan area shall be coordinated with Pacific Gas and Electric, Pacific Bell, and Falcon Cable TV of Northern California.

Policy 7.5.2: All new utility lines in the project area shall be placed underground.

Policy 7.5.3: Public utilities, such as transformers, terminal boxes, meters, fire risers, backflow preventers and other similar facilities, shall be screened and oriented away from public view except as required by City or public utility companies.

Policy 7.5.4: All street lighting shall be designed to utilize energy efficient illumination devices and shall be of the minimum intensity necessary to provide for the safety needs of motorists and pedestrians. The placement of street lighting shall be limited to only those locations warranted.

7.2 WATER

The entire West Fairview Road Specific Plan area is within Zone 3 of the Sunnyslope County Water District service area. The nearest district reservoir is located east of Fairview Road approximately 3,000 feet northeasterly of the northern Plan boundary. The reservoir has a storage capacity of 2,000,000 gallons.

The proposed water infrastructure plan for the Plan area is illustrated in Figure 7.1. An existing district transmission main system nearly surrounds the West Fairview parcels. The Specific Plan water system infrastructure would connect to existing 12 inch mains in Fairview Road, Airline Highway and Sunset Drive, and an 8 inch main west of the Plan area in Calistoga Drive. This will complete a full grid system, thereby improving water supply capabilities to the Specific Plan area and existing City and County homes.

Development of the West Fairview Road Specific Plan area could yield up to 1594 dwelling units. At an average water demand of 360 gallons per day (gpd), per single family dwelling unit, the water demand of the Plan area would be approximately 625 acre-feet per year. Peak demands, fire abatement activities and landscaping needs would increase this total slightly. The Sunnyslope County Water District can supply this demand in an average year with existing facilities. The City of Hollister Fire Department water flow standard of 1500 gallons per minute, and 20 psi residual pressure, can also be met with present system facilities.

7.3 SANITARY SEWER

Effluent from the Specific Plan project will be treated at the City's domestic wastewater treatment facility. Completed in 1980, the treatment facility is located west of Hollister, on the north side of Highway 156. The present licensed capacity of the plant is 2.69 million gallons per day (mgd). Currently, 2.2 mgd of effluent is treated at the facility. The maximum one-month flow recorded in 1988 was 2.3 mgd.

The City's Public Works Master Plan specifies a series of future improvements which would increase the capacity of the sewage treatment plant to 5.22 mgd by the year 2004; plant expansion to approximately 3.6 mgd is planned within five years. The City is presently processing a request for the increase in the plant's licensed capacity with the California Regional Water Quality Control Board. Funds have been secured to purchase land adjacent to the treatment plant for future expansion.

Although the Specific Plan area is adjacent to the City of Hollister's city limits and existing urban neighborhoods, no municipal sewer mains with excess capacity are immediately available for extension to service the Plan area. In anticipation of the development of the Specific Plan vicinity, the City of Hollister Public Works Master Plan includes improvement plans for the construction of a 15-inch sewer main within Airline Highway. This main would travel north across Airline Highway, turn west across vacant properties on the west side of Airline Highway, turn north up Southside Road, then turn west again through a subdivision that is currently being constructed. The main will eventually connect at an existing major trunk line that now terminates near East and South Streets. Development of the entire wastewater collection line from Airline Highway through to the trunk line at East and South Streets would be required to service the Specific Plan area.

FIGURE 7.1

Water Infrastructure Improvement Plan

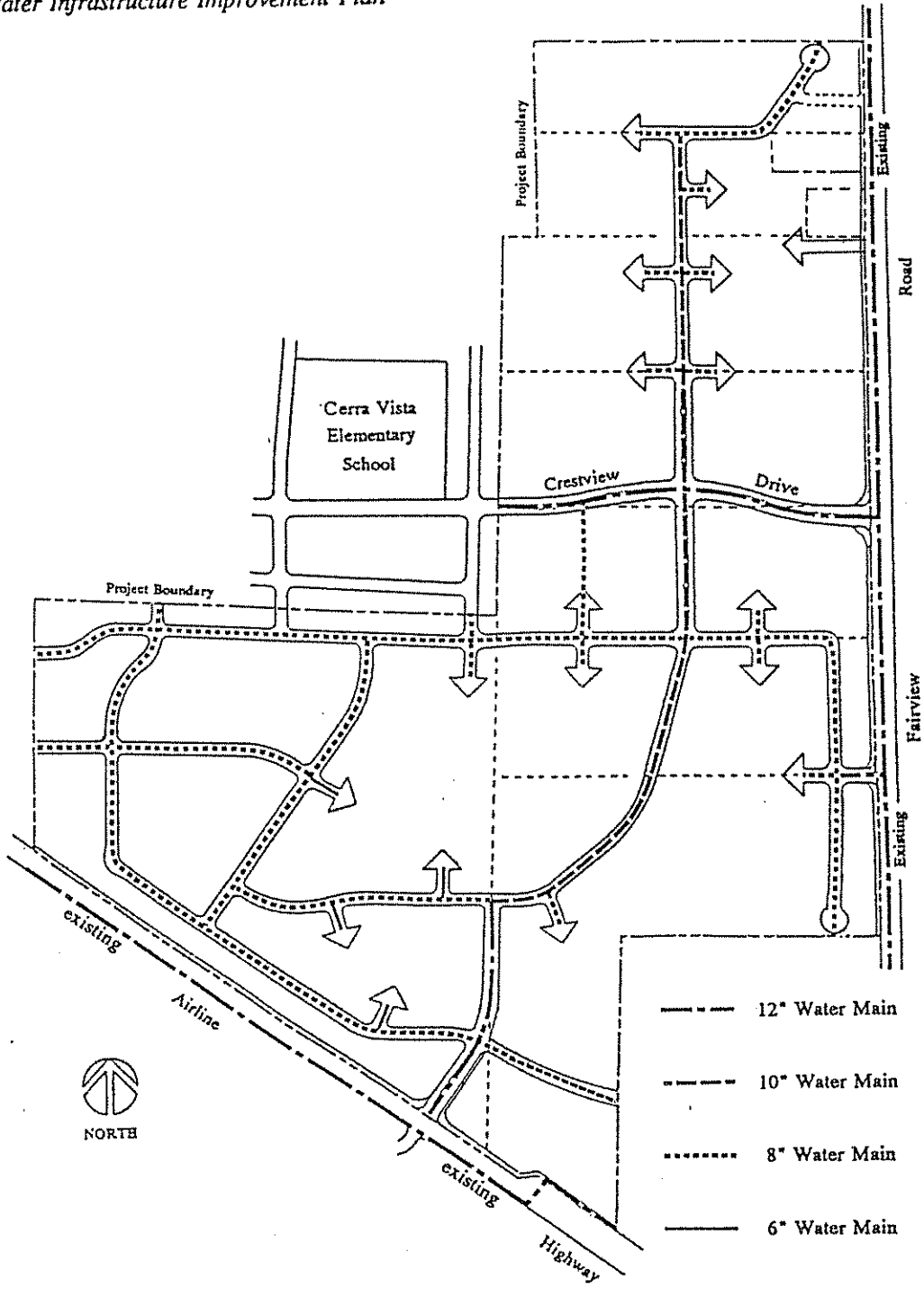
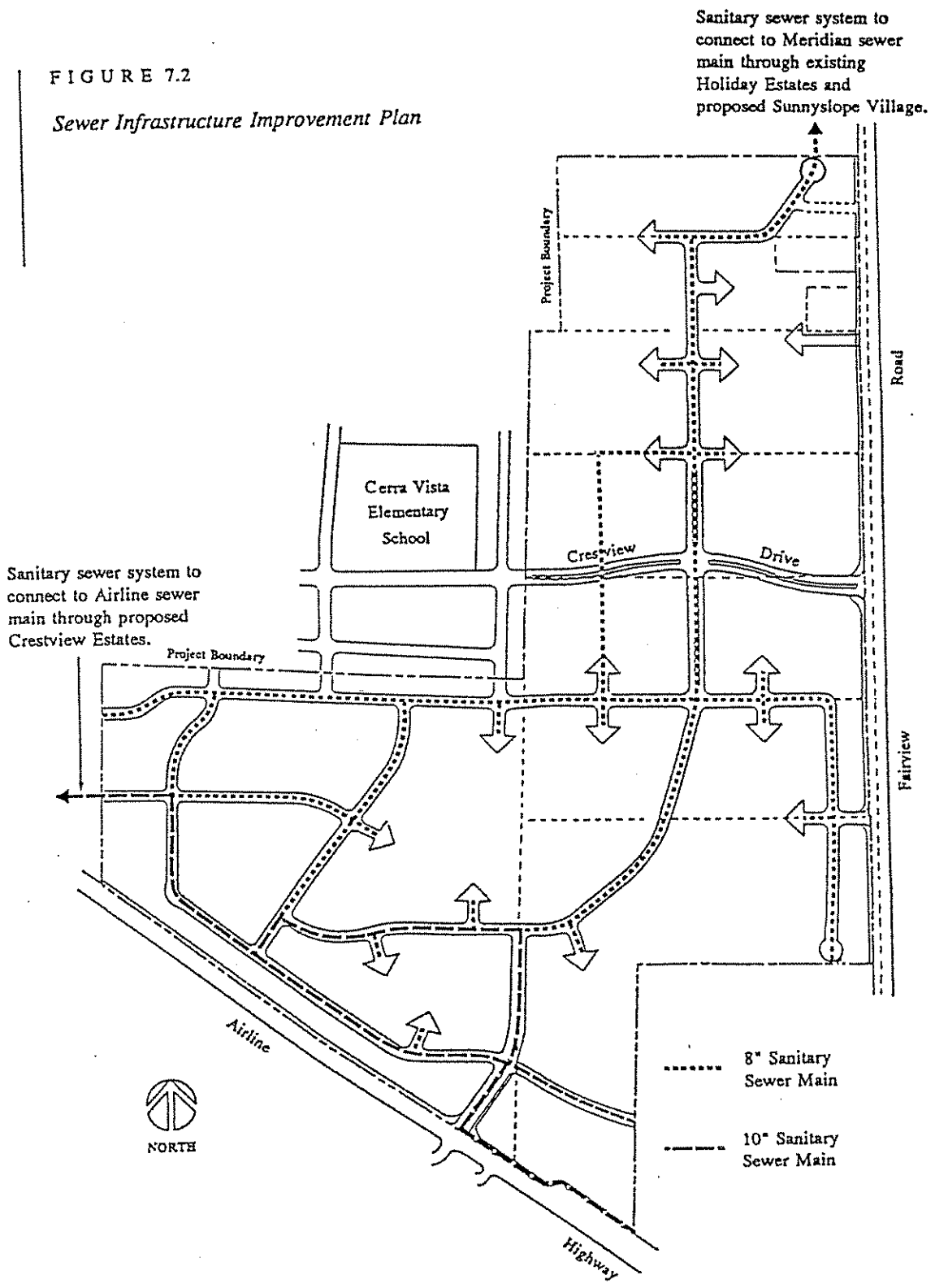


FIGURE 7.2

Sewer Infrastructure Improvement Plan



Sanitary sewer system to connect to Airline sewer main through proposed Crestview Estates.

Sanitary sewer system to connect to Meridian sewer main through existing Holiday Estates and proposed Sunnyslope Village.



- 8" Sanitary Sewer Main
- 10" Sanitary Sewer Main

Sewer service to the northerly portion of the site would be provided via either of the following solutions:

- Via gravity flow, effluent flow would travel to the north in an offsite extension through Holliday Estates and the proposed Sunnyslope Village development. The extension would connect into an existing sewer main in Meridian Street. Improvement of the Meridian Street main would be required to accommodate flows generated by the Specific Plan project¹; or
- A pump station and force main to transfer effluent flow to a proposed Airline Highway gravity system.

The southern portion of the Plan area could be serviced via gravity flow by the proposed Airline Highway wastewater collection line. In the event that sewer infrastructure associated with development of parcel A9 (refer to Figure 1.4) is not constructed at the time of development of parcels A7 and A8, portions of Parcel A8 and A7 that naturally drain to the south instead of the north could be serviced via an interim pump and force main to transfer effluent flow to the northern gravity system described above. Once sewer infrastructure associated with parcel A9 is completed, effluent flow from the southern portions of parcel A8 and A7 would utilize that gravity flow system.

A detailed engineering study will be required in conjunction with the development of the northern parcels to determine the most economical and feasible solution to providing sanitary sewer service to the northern portion of the Specific Plan area. The feasibility of these options is also dependent on the ultimate phasing of sewerage construction.

Figure 7.2 illustrates the infrastructure improvements within the Specific Plan area required to accommodate effluent flows, via gravity, generated by the project. Based on an average generation factor of 290 gallons per day per single family dwelling unit, at buildout the 1594 unit Specific Plan project would generate .46 mgd. As previously discussed, the Hollister wastewater plant is currently treating 2.20 mgd, whereas the plant capacity is 2.69 mgd. In addition, plant expansion to approximately 3.6 mgd is planned within five years. Therefore, given the projected ten year buildout of the Specific Plan area, no treatment plant capacity constraints are anticipated.

7.4 STORM DRAINAGE

Existing Conditions

The topography of the Specific Plan properties divides the area into two distinct drainage basins. The southwesterly 128 acres drains toward Airline Highway, and thence to the San Benito River.

¹Improvements to the Meridian Street Main are currently being pursued by the City of Hollister.

The remaining 168 acres drains northerly to a tributary of Santa Ana Creek. As illustrated on Figure 7.3, the Santa Ana Creek tributary crosses the northeasterly portion of the Plan area in a well defined meandering channel. The upstream drainage crosses Fairview Road at John Smith Road, and flows through a 6' wide by 5.5' deep box culvert onto the site. The culvert capacity has been substantially reduced by siltation.

A recent flood plain study prepared for the Federal Emergency Management Agency (FEMA) included the Santa Ana Creek tributary through the site. The upstream limit of this study on the Santa Ana Creek Tributary was Fairview Road. The study, presently under review by FEMA, indicated that the tributary is subject to flooding within the existing channel because the culvert is too small to handle a 100-year flow under its current configuration. To mitigate flooding of the tributary, culvert improvements could be installed to increase its capacity, or Fairview Road could be raised to provide the necessary head for the culvert to pass the 100-year flow.

A similar condition exists with one of two corrugated metal pipe (CMP) culverts installed in Sunset Drive, a residential roadway of Holliday Ranch Estates. A 72" CMP culvert was installed to pass the flow from the Santa Ana Creek tributary, and a 36" CMP culvert was installed to pass the flow from a minor tributary which collects runoff from the Specific Plan properties and some adjacent lands to the west (approximately 200 acres). Based on the FEMA study, an increase in the capacity of the 72" CMP culvert is required to pass a 100-year flow under existing conditions.

At the common boundary of parcels A2 and A3 (see Figure 1.4) a 24" storm drain was installed in conjunction with the improvements for the Walnut Park development to the west. This drain system serves approximately 35 acres to the west of the Specific Plan area. The preliminary infrastructure plans developed for the Plan area indicate that the storm drain will not require relocation; however, the outfall portion will need to be revised and connected to the proposed West Fairview system.

Proposed Drainage System

A preliminary drainage analysis was completed on a major portion of the Specific Plan area using the Rational Formula method. The analysis was based on standard San Benito County and City of Hollister drainage criteria and a ten-year design storm. The estimated pipe sizes shown on Figure 7.3 were determined using this method.

San Benito County requires new developments to provide detention facilities for storm generated runoff from developed areas that exceed amounts generated in the "undeveloped" condition. Based on Soil Conservation Service methods for estimating detention storage requirements, detention ponds will be placed on each of the Specific Plan parcels. Post-development runoff collected in the detention ponds will be percolated and/or released into the Santa Ana Creek tributary once storm activities have ceased.

The size and location of each detention pond is based on individual parcel boundaries. The ultimate size and location of detention ponds will depend upon the final site plan and possible

phasing of development. As discussed in previous Sections of this Specific Plan, the detention pond areas will also serve as recreational/open space. Detention ponds are connected by a linear parkway which also minimally serve as a storm drainage channelization system.

Proposed Specific Plan storm drain improvements for the Santa Ana Creek drainage system also includes intercepting the runoff from the tributary to Santa Ana Creek with a 72" storm drain, at the outlet of the box culvert on Fairview Road. The drain would then run parallel to Fairview Road to the northeast corner of the Plan area, turn west to run along the Plan's northern boundary, and finally deposit into the existing Santa Ana Creek tributary. This storm drain thus eliminates the existing Santa Ana Creek channel through the Specific Plan area.

The system of detention ponds will accommodate storm drainage runoff for the northern 168 acres of the Specific Plan area. To service the southwesterly 128 acres, either of the following improvements would be necessary:

- Runoff from the entire southern drainage basin may be collected and dispersed into an existing unused County quarry site located adjacent to and westerly of Airline Highway. The details of design, timing and costs are currently being studied by the City of Hollister and local engineers. As an alternative to the quarry site, an off-site drain trunk system may be constructed in Airline Highway; or
- In the event that development of parcel A9 does not precede development of Parcels A1 through A8, runoff from parcels A1 through A8 would be collected in a system of detention ponds on site to be percolated and/or released into the Santa Ana Creek tributary once storm activities have ceased. Since a portion of Parcel A8 and A7 naturally drain to the south instead of the north, runoff from these areas would be accumulated in a retention/detention pond and pumped during peak overflow periods to enter the northern system on an interim basis. Once storm drainage infrastructure associated with parcel A9 is completed, runoff from the southern portions of parcel A8 and A7 would utilize that storm drainage system.

These options will be subject to further engineering studies and review by the City Engineer. The feasibility of these options is also dependent on the ultimate phasing of infrastructure construction.

Figure 7.3 illustrates storm drainage infrastructure improvements required if the quarry site alternative is selected.

7.5 POLICE AND FIRE PROTECTION

The entire Specific Plan area is proposed for annexation to the City immediately following adoption of this Specific Plan. Consequently, police and fire protection services will be provided to the Plan area by the City of Hollister Police and Fire Departments, respectively.

The City of Hollister Police Department consists of 22 sworn officers. Based on an ideal officer/population ratio of 1/1000, the Department is currently understaffed by five officers. The Department has a single station located downtown on Fourth Street. Emergency response time to the site would be approximately five minutes. Buildout of the Plan area will generate a demand for approximately four to five new deputies. Land for a fire sub-station within the West Fairview Road project site will be reserved for city use under an agreement to be developed similar to the one negotiated with the school district.

The City of Hollister Fire Department has 10 full-time staff and 18 volunteers available to respond to emergency situations. Like the Police Department, the Fire Department is understaffed by about five persons, based on an ideal staff/population ratio of 1/1500. The Department has a single station located downtown at 110 Fifth Street, with five fire trucks available. The Fire Department is actively pursuing funding for a second fire station. Buildout of the Plan area will generate a demand for three new fire fighters.

The Fire Department will require that a water supply and distribution system be developed for the Specific Plan area that meets fire protection standards. As discussed in Section 7.2, the Sunnyslope County Water District can supply a water flow to the Plan area that meets the City of Hollister Fire Department water flow standard of 1500 gallons per minute, and 20 psi residual pressure.

Per City adopted resolutions, police and fire protection service impact fees are collected on all new residential constructions. A fee of \$0.25 per square foot is collected by both the City of Hollister Police Department and the City of Hollister Fire Department. Assuming an average dwelling unit size of 1,800 square feet, and 1,594 units proposed for construction, approximately \$.71 million in impact fees will be collected by each department.

7.6 PUBLIC SCHOOLS

The West Fairview Road Specific Plan area is within the Hollister Elementary School District (grades K - 8), and the San Benito Joint Union High School District (grades 9 - 12). Both districts serve the City of Hollister, as well as portions of unincorporated San Benito County. The Hollister Elementary School District consists of six elementary schools, whereas the High School District contains one high school.

The six elementary schools provided by the Hollister Elementary School District are not neighborhood serving. Instead, grade levels are allocated among the District's schools to desegregate the ethnic mix within the City. Each school provides three or four grade levels, in which children are bussed to the closest school providing the required grade level and balanced ethnic mix. The current capacity of the District's six schools is 4,170 students. The 1990/1991 enrollment was 4,097 students.

San Benito High School provides the public high school education needs of the City of Hollister and northern unincorporated San Benito County. The capacity of the campus was recently increased to 2,100 students. The 1990/1991 enrollment was 1,682 students.

Both Districts estimate student enrollments generated from new development by applying standard yield rates, developed by the State of California, to the number of housing units. For example, a single family dwelling unit will generate about 0.57 elementary-age children and 0.23 high school-age students. Table 7.1 illustrates the maximum number of students generated from buildout of the Specific Plan area with only single family units.

TABLE 7.1 - STUDENT ENROLLMENT PROJECTION AT FULL BUILDOUT *

<u>Grade Level</u>	<u>Yield Rate</u>	<u># of SF Dwelling Units</u>	<u># of Students</u>
K - 8	0.57	1,594	909
9 - 12	0.23	1,594	366
TOTALS	0.80		1,275

* Full buildout of the Specific Plan area is anticipated to take 8 to 10 years.

To determine the number of classrooms required to serve future students, optimum student to student/classroom ratios are used. The Hollister Elementary School District applies a student/classroom ratio of 30:1, whereas the San Benito High School District uses a 28:1 ratio. Table 7.2 summarizes the classrooms required per district to service the students generated by full buildout of the Specific Plan area.

TABLE 7.2 - PROJECTION OF CLASSROOMS AT FULL BUILDOUT *

<u>Grade Level</u>	<u># of Students</u>	<u>Classroom Ratio</u>	<u># of Classrooms</u>
K - 8	909	30:1	30.3
9 - 12	366	28:1	13.1

* Full buildout of the Specific Plan area is anticipated to take 8 to 10 years.

School districts are allowed to charge up to \$1.65 per square foot of residential development. However, project proponents have entered into an agreement with the school district to pay additional school impact fees in order to fully mitigating school impacts. Assuming an average dwelling unit size of 1,800 square feet, and 1,594 units, a total of approximately \$11.4 million in school impact fees would be collected. School district fees are divided proportionally between the two school districts.

In addition, up to ten acres of property shall be reserved in the Specific Plan area for the future construction of an elementary school site.

7.7 SOLID WASTE DISPOSAL

Solid waste generated within San Benito County and its incorporated cities is disposed of in the City and County Landfill. The 65-acre landfill is located on John Smith Road, approximately two miles east of the Specific Plan area.

In cooperation with the City and County, curbside pickup and recycling shall be encouraged within the Specific Plan area.

7.8 UTILITIES

Future public utilities to be provided to the Specific Plan area include electricity, natural gas, telephone, and cable television service. Electricity and natural gas services are provided to all of Hollister and San Benito County by Pacific Gas and Electric Company (PG & E). Telephone service is currently provided to all City and County residents by Pacific Bell. Falcon Cable TV of Northern California provides cable television service.

Electricity, natural gas, and telephone services are presently provided to the few existing homes within the Specific Plan area, via electric power and telephone lines extending along Fairview Road.

The buildout of the Specific Plan area will place significant demands on the existing transmission and distribution systems of the noted utility services. The delivery of utility services to individual developments will need to be reviewed by PG & E, Pacific Bell, and Falcon Cable TV of Northern California, as individual project proposals are processed.

SECTION 8.0

IMPLEMENTATION

California Planning and Zoning law requires that a specific plan identify a program of implementation measures including regulations, programs, public works projects and financing measures necessary to carry out the plan. The major components of this Implementation Element include a discussion of regulatory and zoning changes; the addition of new or augmented review and permitting procedures; recommendations for financing mechanisms to fund planned improvements; approaches for ensuring on-going maintenance of specific services and special areas; and strategies which anticipate the possible sequence or phasing of growth. These are discussed in greater detail below.

8.1 REGULATORY CHANGES

Zoning Ordinances

This Specific Plan serves as a basis for changes to existing City zoning to allow for the development of a variety of residential densities, with supporting recreational/open space uses, within the Plan area. To carry out the purposes of the Specific Plan, these zoning changes shall be adopted by City ordinances in the form of a Planned Development Overlay Zone applied throughout part or all of the project area, as appropriate.

Once a specific plan has been adopted, no local public works project, tentative subdivision map or zoning ordinance can be undertaken for the plan area that is in conflict with the specific plan. Whenever the provisions of this Specific Plan conflict with the provisions of the zoning ordinance or whenever the provisions of the zoning ordinance reflect an internal conflict, the Specific Plan shall govern.

Building, Health and Safety Codes

The development standards set forth in this Plan are not meant to take the place of City building, health, and safety codes. No permit shall be issued for development that does not meet the requirements of these regulations.

Procedures for Specific Plan Amendment

Pursuant to Section 65453 of the California Government Code, a specific plan may be amended through the same adoption procedures used for a general plan. However, a specific plan may

be amended as often as deemed necessary by the City Council. If the City General Plan is amended so that a specific plan no longer conforms to it, the specific plan also must be amended.

8.2 REVIEW AND PERMITTING PROCEDURES

All discretionary and administrative entitlements granted for projects in the Plan area must be found to be consistent with the Specific Plan to ensure conformance with legal requirements.

Design Review

The West Fairview Road Specific Plan modifies the existing City review and permitting procedures by providing clearly defined policies, implementation actions and development standards to which projects proposed within the Plan area must conform. All such projects will be subject to the review and approval by the appropriate City design review body, and must be determined to be consistent with this Plan in order to obtain project approval.

Environmental Review

Adoption of a specific plan is a project subject to California Environmental Quality Act (CEQA) guidelines. An Environmental Impact Report (EIR) covering the impacts of this Specific Plan is being prepared and will be certified at the time of Specific Plan approval. The EIR covers the impacts of the proposed mix and intensity of residential and supporting recreational/open space uses.

Further environmental review or negative declaration shall generally not be required for residential projects, including any land subdivision or zoning change, which are undertaken pursuant to and in conformity with this Specific Plan. However, a supplemental EIR may have to be prepared, after the West Fairview Road Specific Plan has been adopted, if one or more of the following conditions applies.

1. The project is substantially different from the mix, intensity or type of use described in the Specific Plan;
2. Significant changes to the site or surrounding areas have occurred since the adoption of the Specific Plan; or
3. Additional information about the potential impacts of the project becomes available after the EIR has been certified.

8.3 CAPITAL IMPROVEMENTS PROGRAM

This Specific Plan identifies infrastructure facilities needed to serve future development. Infrastructure facilities required include the construction of new roadways, water, sewer, and storm drainage facilities and public utilities. In addition, as described in Section 4.3, the widening of Airline Highway and Fairview Road would also be required.

Engineering studies prepared by MacKay & Soms, project engineers, have estimated the preliminary costs for mutually beneficial infrastructure improvements, i.e. those improvements that are required to service several or more Specific Plan parcels. As summarized in Table 8.1, the total cost of the infrastructure improvements necessary to develop the site is estimated to be about \$10.0 million. This cost is in addition to any parcel specific costs or City-wide facility requirements, such as those covered under impact fee programs.

TABLE 8.1 - ESTIMATED COSTS

<u>Costs</u>	<u>Improvements</u>
INFRASTRUCTURE:	
	Surface Improvements \$2,616,300
	Sanitary Sewer Systems ¹ 1,590,400
	Storm Drain Systems 2,164,100
	Water Systems 374,900
	PG & E Systems 309,800
SOFT COSTS & CONTINGENCIES:	
	Agency Fees ¹ 455,000
	Engineering ¹ 791,000
	Contingencies & Miscellaneous ¹ 1,699,500
	<hr/>
TOTAL	\$10,001,000

¹ Includes Sunnyslope Village sewer extension.
Source: MacKay & Soms, Project Engineers, 1991.

The largest cost items are surface improvements, which include the street and frontage requirements for urban development. The majority of the surface costs apply to interior streets, and the associated curbs, gutters and landscaping. The additional off-site costs pertaining to widening and frontage dedications for Airline Highway and Fairview Road are also included.

As discussed in Section 7.0, it is proposed that the Specific Plan area will be serviced by existing City sanitary sewer and storm drainage systems. The sewer and drainage improvements required to support development in the Specific Plan area include on-site, as well as off-site improvements. Off-site improvements are necessary since collection facilities must be extended to the project site.

Water service is available at the project boundaries. The water system improvement costs are therefore limited to those required to deliver water on-site. The PG&E system cost refers to the undergrounding of existing overhead electric lines. The amount shown is net of costs refundable from PG&E. Soft costs and contingencies for the total infrastructure requirements include engineering and agency fees, contingencies at 15 percent of construction costs, and bond and insurance premiums at one percent of construction costs.

8.4 FINANCING

The financing program identifies sources and methods of funding for the infrastructure and public facilities required to serve new development in the West Fairview Road Specific Plan area. The objective of the plan is to permit the on-site and community wide requirements to be satisfied by utilizing the most appropriate techniques available, while placing a reasonable burden on the developers and eventual home buyers. It is also important that the required costs are equitably shared among the individual developers of the Specific Plan area. The program assumes that three techniques may be employed in the financing of public improvements: developer financing, impact fees, and a Mello-Roos district.

In line with traditional practices, it is possible that a Mello-Roos Community Facilities District (CFD) could be used to finance a significant portion of the major off-site infrastructure improvements, particularly those required at the project onset (mutually beneficial). Developer dedication and financing could be used to meet the construction funding requirements for the remaining on-site improvements. Community wide facilities not specifically identified within the Specific Plan area could be funded through the applicable impact fees imposed by the City of Hollister. These would be paid by the individual developers as building permit issuance takes place. In the event that the Mello-Roos CFD bonding capacity is not exhausted after the major infrastructure requirements have been met, a portion of the on-site improvements and/or development fees could also be financed through the Mello-Roos CFD. The preliminary financing program is summarized as follows:

1. Mello-Roos special district financing could be used for major off-site improvements required prior to development (mutually beneficial).
2. Developer dedication and financing could be used to provide on-site, parcel specific improvements.
3. Development impact fees could be used for city-wide facilities to accommodate regional growth.
4. Any Mello-Roos Community Facilities District bonding capacity not used for major off-site infrastructure or other City requirements could finance on-site improvements or impact fees.

8.5 MONITORING AND MAINTENANCE

On-going City Services

If necessary, an ongoing special tax, as authorized under the Mello-Roos Community Facilities District Act and approved by landowners in the affected planning area, should be levied to readdress any fiscal imbalance that may result from changes in the assessment of unit costs for services or reductions in the overall development program. A fiscal analysis should be prepared based upon the adopted Specific Plan and on estimates of cost of services, and if the analysis indicates a fiscal imbalance would be produced, then an additional condition of development approval would be imposed. This tax, which would augment property and sale taxes and other revenues generated by land uses in the Specific Plan area, could be designated for public schools and police and fire protection services. An annual update of the fiscal analysis could be prepared to determine the amount that would produce a fiscal balance; the tax rate could be set annually by the Hollister City Council.

Landscape and Lighting

A Landscape and Lighting Maintenance District shall be established so that a benefit assessment sufficient in amount to ensure the maintenance of the recreational/open space and landscape easement areas can be levied. The Lighting and Landscaping Act of 1972 enabled assessments to be imposed in order to finance acquisition of land for parks, recreation and open space areas, installation of construction of park improvements and maintenance and servicing of any of the above. The assessment must be evaluated annually to ensure that it is sufficient to cover project costs. In this Specific Plan, a Landscape and Lighting Maintenance District would be targeted chiefly for providing funds for the maintenance of park and recreation areas and landscape easements. The City of Hollister and the Project developers must enter into a Lighting and Landscaping Maintenance Agreement prior to Tentative Map approval.

8.6 DEVELOPMENT PHASING

The project proponents will propose a phased implementation program that will result in a build out of the project over a period of years. Residential allocations for the plan area will be identified in the project Development Agreement to be approved in conjunction with, or immediately following adoption of the Specific Plan. The final phasing plan will also be incorporated into the project Implementation Program.

Phasing for Parcel A9 is the subject of an existing development agreement between the City of Hollister and the owner. That development agreement specifies allocation of a total of 629 units, distributed among Parcel A9 and a 35-acre parcel immediately west. Development is to be phased over a nine-year period, with 60-75 allocations granted per year. If the permitted maximum of allocations is not issued in any one year, the remaining amount may be carried over to succeeding years providing that the maximum number of permits issued for any year does not exceed 105 units. Additionally, if the development agreement expires for the subject property, the implementation program may need to be amended to accommodate Parcel A9 if necessary.

In order for residential development to occur in the City of Hollister, a project site must be located within the City's Residential Development Service Area (RDSA). The RDSA is defined by a boundary. With the exception of Parcel A9, the Specific Plan area is currently outside of, but contiguous to, the RDSA boundary.

Since sewer and storm drainage services will be provided to the Specific Plan area by the City, annexation to the RDSA will be required for each Plan parcel. For RDSA annexation to occur, the City requires applicants to demonstrate that services can be provided to the subject property. Site serviceability from both an engineering and financial perspective is required. Components of this Specific Plan illustrate the manner in which services will be provided to the site (see Section 7.0, Public Services and Utilities, and Section 8.4, Financing Program). Therefore, annexation of the Specific Plan area to the RDSA will occur in conjunction with, or immediately following, the adoption of the West Fairview Road Specific Plan.

Infrastructure

The phasing of infrastructure construction is instrumental in providing services to each of the Specific Plan parcels. Although the final infrastructure construction schedule has not been formulated at this time, the completion of certain major infrastructure items (mutually beneficial) must precede development of the site. For example, off-site sewer and drainage improvements must be constructed to extend service to the Specific Plan area at the project onset. Major circulation, water and utility improvements may also be necessary early in the project.

To construct these improvements, it will be necessary to obtain financing in advance of development. Accordingly, a Mello-Roos Community Facilities District may be considered to create a commitment from all property owners to fund the mutually beneficial project infrastructure. In this manner, construction funding can be secured with a lien against the

property prior to development of the site. Also, the first year of special tax payments can be deferred for up to two years under the Mello-Roos procedure. This will defer the additional tax burden carried by vacant land through the infrastructure construction period.

Financial Limitations

Development phasing of the Specific Plan area will also be affected by the limitations imposed by the financial markets. Since Mello-Roos financing is secured by liens on the properties within the Communities Facilities District, the amount that can be borrowed will be constrained by district property values. The financial markets require that the bond amount will not exceed approximately 25 to 35 percent of the anticipated market value of the improved land in the district after the improvements have been put in place. In addition, from the standpoint of marketability of new homes, the special tax burden on future home buyers should be limited to less than one percent of assessed value.

8.7 IMPLEMENTATION OF THE WEST FAIRVIEW ROAD SPECIFIC PLAN

The primary steps to implement the West Fairview Road Specific Plan are listed below. A discussion of each implementation step follows:

Steps Implementing the West Fairview Road Specific Plan*

- (1) Certify EIR and adopt Specific Plan. Approve Development Agreement between City and Specific Plan property owners. Prezone and initiate annexation to the City. Annex Specific Plan area to RDSA.
- (2) Prepare implementation program.
- (3) Prepare annexation agreement between the City and County.
- (4) Complete detailed engineering studies for major off-site and on-site infrastructure facilities.
- (5) Form a Financing District for major off-site improvements required prior to development.
- (6) Prepare and Process Individual Tentative Maps.

* Concurrent preparation/processing of each noted step can occur if desired.

1. **Certify EIR and adopt Specific Plan. Prezone and initiate annexation. Annex Specific Plan area to RDSA. Approve Development Agreements between the City and Specific Plan property owners.**

Adoption of the Specific Plan, Certification of the EIR and approval of a Development Agreement between the City and West Fairview Road Property Owners shall occur simultaneously. Initiation of annexation of the Specific Plan area to the City of Hollister and project pre zoning shall also occur at that time, or immediately thereafter. Annexation of the Specific Plan area to the Hollister Residential Development Service Area (RDSA) will occur in conjunction with Specific Plan adoption, since sewer and storm drainage services will be provided by the City.

2. **Implementation Program.**

A detailed Implementation Program will be prepared identifying, prioritizing and phasing the various steps implementing the Specific Plan. The Implementation Program should address the following:

- (a) The allocation process;
- (b) Applicable financing programs;
- (c) The affordable housing program;
- (d) The phasing and construction of infrastructure improvements;
- (e) Zoning requirements;
- (f) Mitigation monitoring; and
- (g) The tentative map process.

3. **Annexation Agreement**

An annexation agreement between the City and County will be prepared specifying mutually agreeable tax revenue allocations.

4. **Complete detailed engineering studies for major off-site and on-site infrastructure facilities.**

An analysis of infrastructure improvements and cost estimates was completed as part of the Specific Plan program at a preliminary pre-design planning level. More refined analyses and cost estimates are required before a final financing plan can be adopted.

5. **Form a Financing District for major off-site improvements required prior to development.**

It is assumed that tax-free municipal bonds (e.g., Mello-Roos Community Facilities district Bonds) will be used to finance major off-site infrastructure improvements. The proceeds of a bond issue could also be used to finance on-site improvements, depending on

landowner/developer preferences, provided that the total size of the bond issue meets the test of financial prudence described previously.

6. Prepare and Process Individual Tentative Maps.

Each individual property owner will be responsible for the design and processing of a Tentative Map which meets the policy and design criteria defined in the West Fairview Road Specific Plan. Tentative Maps will be processed by the City. Annexation of the entire Plan area will occur prior to Tentative Map approval.

SECTION 9.0

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APPENDIX A
DESIGN STANDARDS

APPENDIX A

DESIGN STANDARDS

Appendix A includes applicable design standards and specifications for the development of the Specific Plan area. The sources of the following details and standards include: City of Hollister Parks and Recreation Master Plan; San Benito County Bikeway Plan; City of Hollister Standard Specifications and Details; City of Hollister Streetscape Improvement Plans; and the Noise Assessment Study prepared by Edward L. Pack Associates, Inc.

**A.1 CITY OF HOLLISTER PARKS AND RECREATION MASTER PLAN, 1989
AND SAN BENITO COUNTY BIKEWAY PLAN, 1991**

Standards for Bikeways and Neighborhood Parks

Definition of Types of Bikeways

Class I Bikeway (bike path)

- Generally, bike paths should be used to serve corridors not served by streets and highways or where right-of-way exists, permitting such facilities to be constructed away from the influence of parallel streets. Bike paths should offer opportunities not provided by the road system. They can either provide a recreational opportunity, or, in some instances, can serve as direct high-speed commute routes, if crossflow by motor vehicles can be minimized. The most common applications are along rivers, ocean fronts, canals, utility rights-of-way, abandoned railroad rights-of-way, within college campuses, or within and between parks. There may also be situations where such facilities can be provided as part of planned developments. Another common application of Class I facilities is to close gaps in bicycle network continuity caused by construction of freeways, or because of natural barriers (river, mountains, etc.). At this time, development of Class I Bikeways is important due to the availability of 116 funding for their construction.

Class II Bikeway (bike lane)

- Bike lanes are established along streets in corridors where there is significant bicycle demand, and where there are distinct needs that can be served by them. The purpose should be to improve conditions for bicyclists in the corridors. Bike lanes are intended to delineate the rights-of-way assigned to bicyclists and motorists and to provide for more predictable movements by each. This can be accomplished by reducing the number of lanes, or prohibiting parking on given streets in order to delineate bike lanes. In addition, other things can be done on bike lane streets in order to make more room for bike lanes and improve the situation for cyclists that might not be possible on all streets (e.g. improvements to the surface; augmented sweeping programs, special signal facilities, etc.).

Class III Bikeway (bike route)

- Bike routes are shared facilities that serve either to: (1) provide continuity to other bicycle facilities (usually Class II bikeways); or (2) designate preferred routes through high-demand corridors. As with bike lanes, designation of bike routes should indicate to bicyclists that there are particular advantages to using these route as compared with alternative routes. This means that responsible agencies have taken actions to assure that these routes are suitable as shared routes and will be maintained in a manner that meets the needs of bicyclists. Normally, bike routes are shared with motor vehicles. The use of sidewalks as Class III bikeways is strongly discouraged.

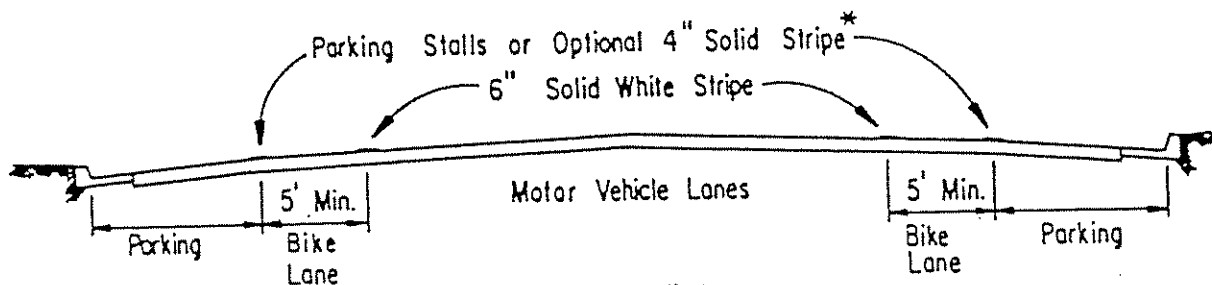
Bikeway / Pedestrian Path

- Minimum 8 feet, maximum 12 feet wide; 12 feet where joint use with pedestrians.

Bikeway Construction

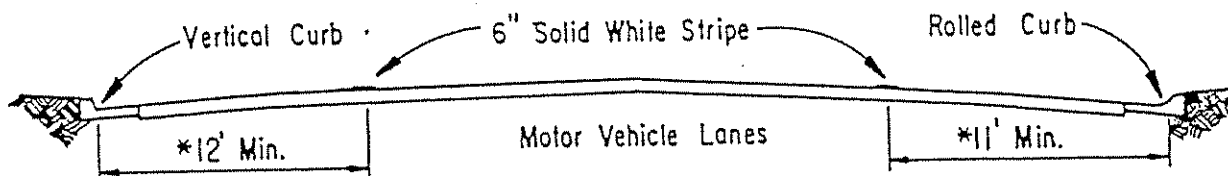
- Asphalt or concrete.

TYPICAL BIKE LANE CROSS SECTIONS
ON 2-LANE OR MULTI-LANE HIGHWAYS



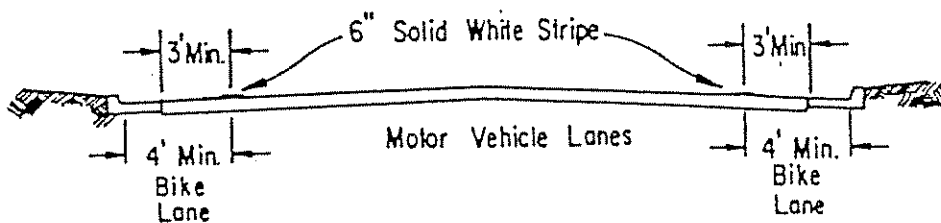
* The optional solid white stripe may be advisable where stalls are unnecessary (because parking is light) but there is concern that motorists may misconstrue the bike lane to be a traffic lane.

(1) STRIPED PARKING

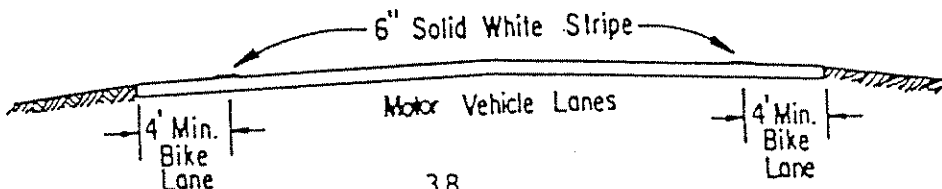


* 13' is recommended where there is substantial parking or turnover of parked cars is high (e.g. commercial areas).

(2) PARKING PERMITTED WITHOUT
PARKING STRIPE OR STALL



(3) PARKING PROHIBITED



(4) TYPICAL ROADWAY
IN OUTLYING AREAS
PARKING RESTRICTED

EXHIBIT 2

HIGHWAY DESIGN MANUAL

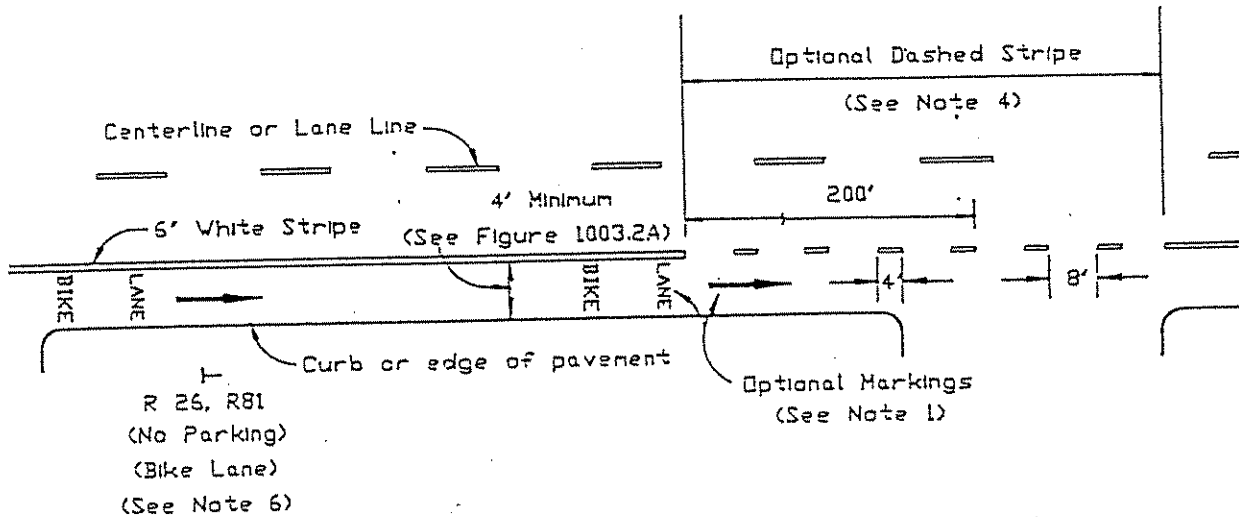
1000-22

July 1, 1990

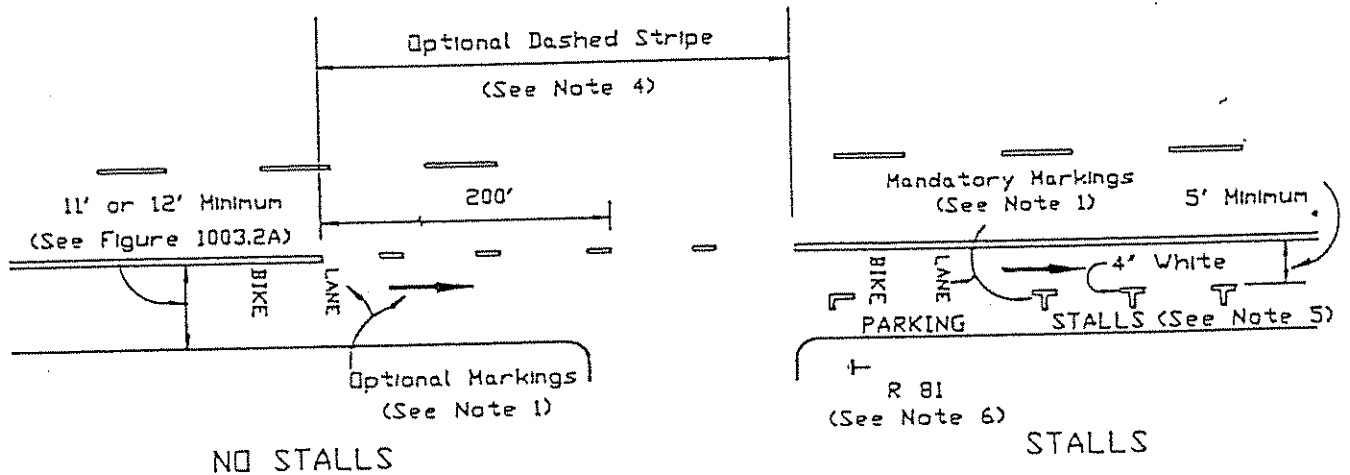
Figure 1004.3

Bike Lane Signs and Markings

WHERE VEHICLE PARKING IS PROHIBITED



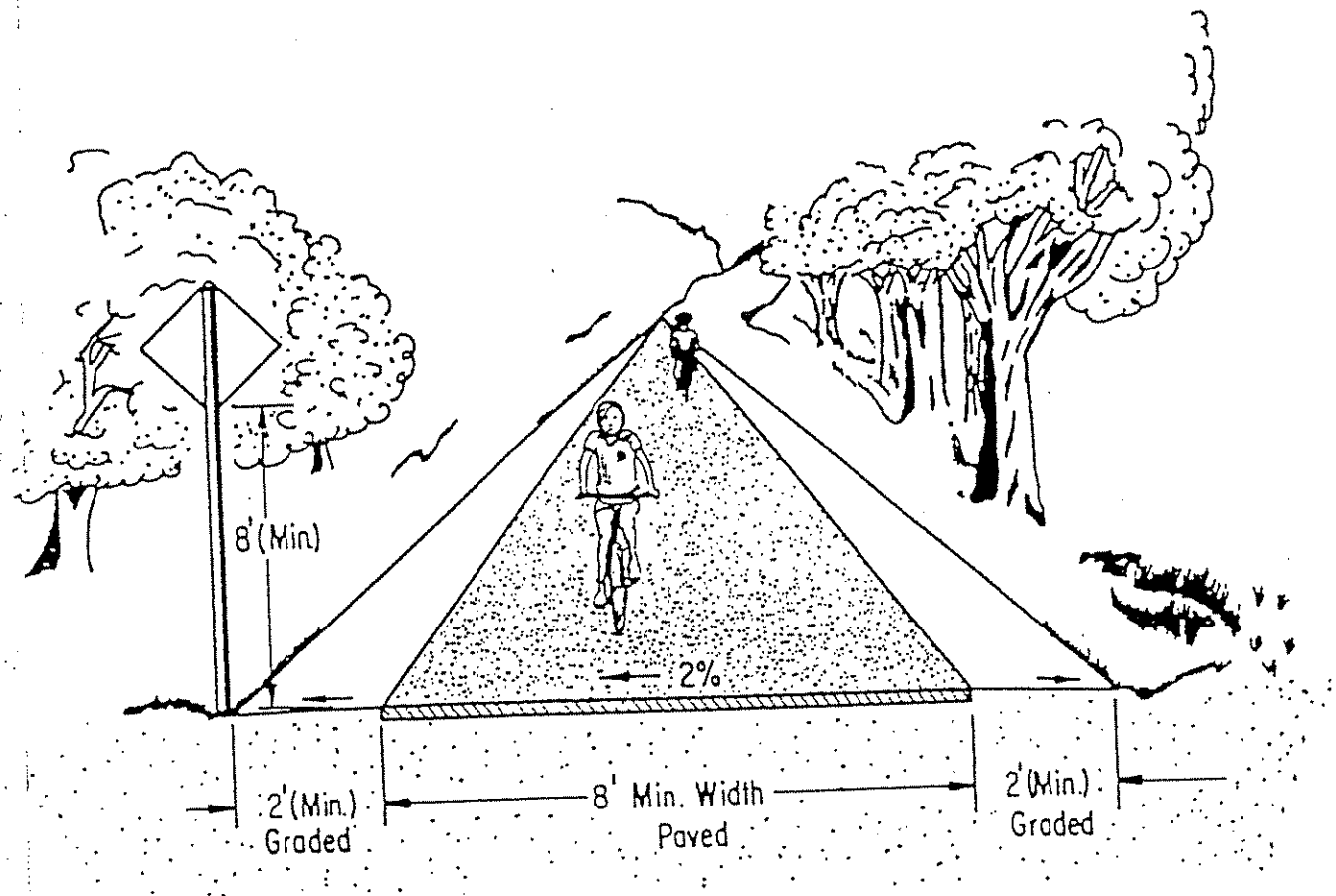
WHERE VEHICLE PARKING IS PERMITTED



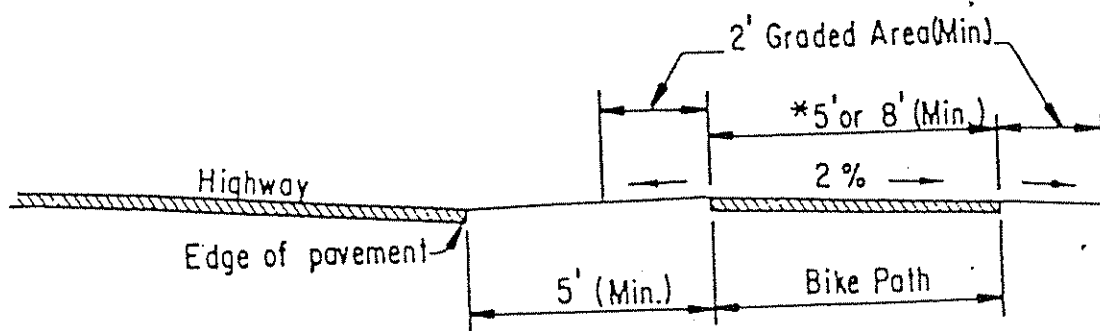
NOTES:

1. The Bike Lane pavement markings shall be placed on the far side of each intersection, and may be placed at other locations as desired.
2. The use of the bicycle symbol pavement marking to supplement the word message is optional.
3. The G93 Bike Route sign may be placed intermittently along the bike lane if desired.
4. The bike lane line may either be dropped entirely, 200' in advance of the intersection, or a dashed line carried to the intersection or through the intersection.
5. In areas where parking stalls are not necessary (because parking is light), it is permissible to paint a 4' solid white stripe to fully delineate the bike lane. This may be advisable where there is concern that motorists may misconstrue the bike lane to be a traffic lane.
6. The R81 bike lane sign shall be placed at the beginning of all bike lanes, on the far side of every arterial street intersection, at all major changes in direction, and at maximum half-mile intervals.

TWO-WAY BIKE PATH ON SEPARATED
RIGHT-OF-WAY



TYPICAL CROSS SECTION
BIKE PATH ALONG HIGHWAY



* One-Way: 5' Minimum Width
Two-Way: 8' Minimum Width

CITY OF HOLLISTER NEIGHBORHOOD PARK STANDARDS

- Size:**
- 3 acres minimum; 5 acres desirable.
 - Minimum 350 feet on one side.
- Acreage Ratio:**
- 2 acres/1000 persons or one park per 3,000-5,000 people.
- Service Area:**
- 1/4 - 1/2 mile radius.
- Site Character:**
- Contiguous useable shape.
 - Access to bicycle and pedestrian trail system.
 - At least 80% level area for active recreation.
 - Should possess some natural or visual qualities, such as existing trees, orchard, etc.
- Access/Location**
- At least 2 public street frontage; 3 or 4 frontages desirable.
 - Located on residential streets.
 - Adjacent to schools where feasible.
- Park Design:**
- Provide an innovative and interesting character or theme.
 - Provide attractive landscaping incorporating large shade and windbreak trees.
 - Provide distinctive entry/focal point.
- Sports Facilities:**
- Open turf for softball and soccer.
 - Tennis, basketball, volleyball, multi-use, 3-wall racquetball and/or ball wall where such facilities are not provided on adjacent school property.
- Play Area:**
- High quality, innovative children's play area with apparatus and sand play.
- Picnic Area:**
- Provide tables in a shaded, secluded area for informal family picnics up to 6 - 8 people.
 - Provide barbecue facilities where appropriate for family-sized groups only.
- Natural Area:**
- Provide a natural area such as a grove of trees or an area planted with California native plants.
- Parking:**
- Provide space for 10 - 20 cars where minimum 2 street frontages are not provided.
 - Provide lockable parking for 6 - 10 bikes.

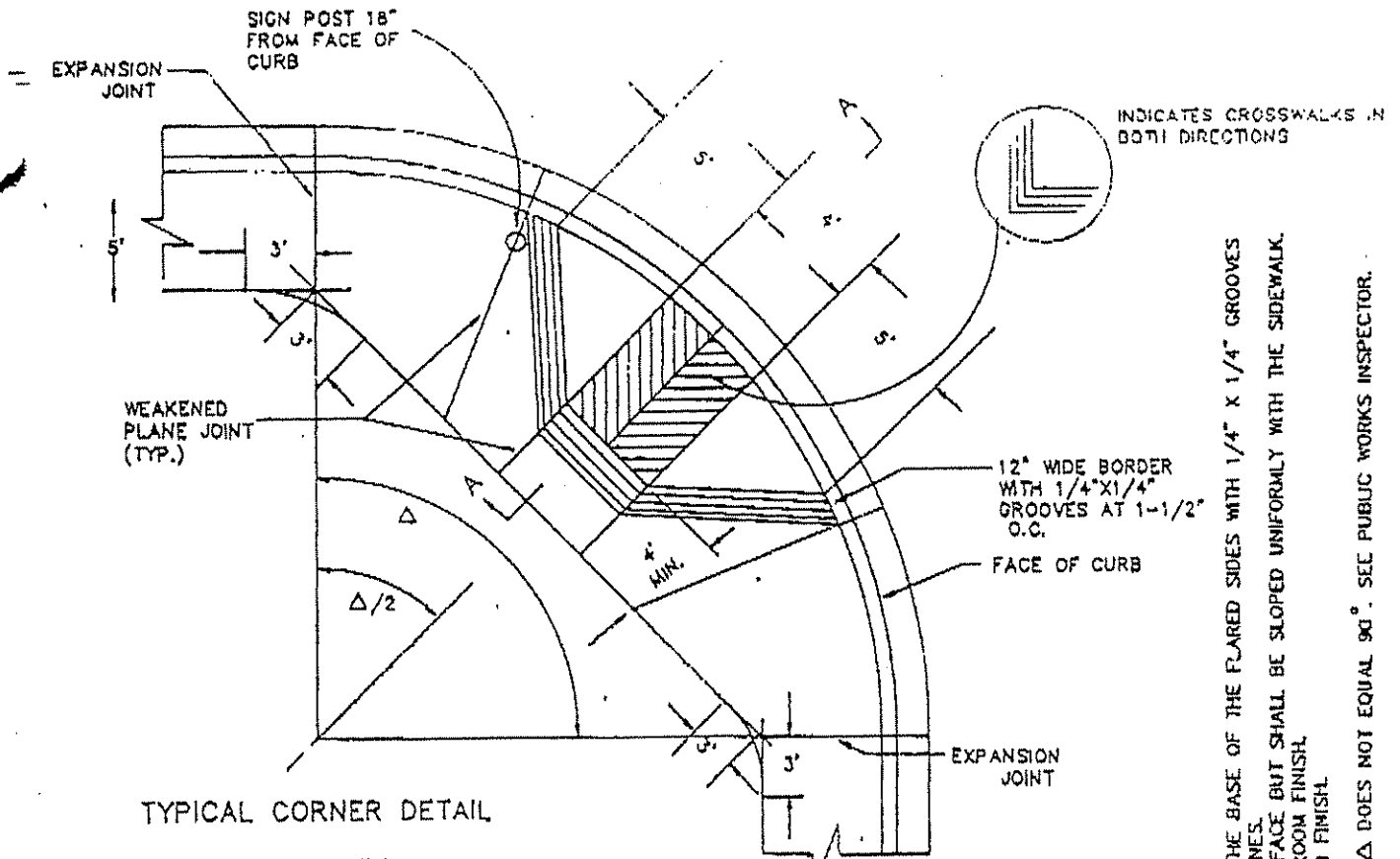
Restrooms:

- Provide portable restrooms; permanent, as feasible.

Lighting:

- No lighting.

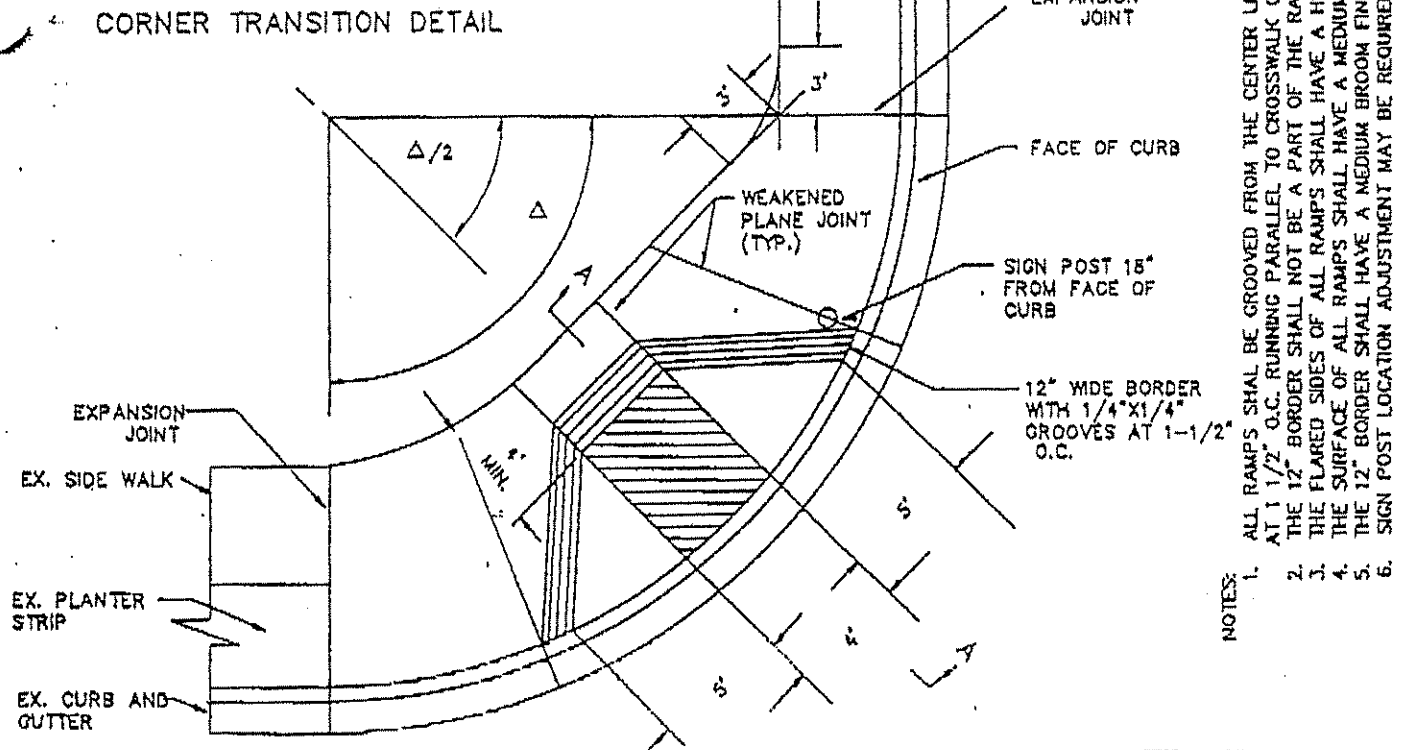
A.2 CITY OF HOLLISTER STANDARD SPECIFICATIONS AND DETAILS



TYPICAL CORNER DETAIL

PLAN VIEW

SEE STANDARD PLAN A-5-2 FOR SECTION A-A.



CORNER TRANSITION DETAIL

- INDICATES CROSSWALKS IN BOTH DIRECTIONS
- NOTES:
1. ALL RAMPS SHALL BE GROOVED FROM THE CENTER LINE TO THE BASE OF THE FLARED SIDES WITH 1/4" X 1/4" GROOVES AT 1 1/2" O.C. RUNNING PARALLEL TO CROSSWALK CENTERLINES.
 2. THE 12" BORDER SHALL NOT BE A PART OF THE RAMP SURFACE BUT SHALL BE SLOPED UNIFORMLY WITH THE SIDEWALK.
 3. THE FLARED SIDES OF ALL RAMPS SHALL HAVE A HEAVY BROOM FINISH.
 4. THE SURFACE OF ALL RAMPS SHALL HAVE A MEDIUM BROOM FINISH.
 5. THE 12" BORDER SHALL HAVE A MEDIUM BROOM FINISH.
 6. SIGN POST LOCATION ADJUSTMENT MAY BE REQUIRED WHEN Δ DOES NOT EQUAL 90°. SEE PUBLIC WORKS INSPECTOR.

CONCRETE SIDEWALK RAMP & CORNER DETAIL

CITY OF HOLLISTER
STANDARD PLANS

APPROVED:

Max H. Bridges
MAX H. BRIDGES R.C.E. 24152- EXP. 12/31/93

DATE	APRIL 30, 1992
REVISED	
DETAIL	A-5-1
SHEET 1 OF 2	

**A.4 NOISE ASSESSMENT STUDY FOR THE WEST FAIRVIEW ROAD SPECIFIC
PLAN PREPARED BY EDWARD L. PACK ASSOCIATES, INC., MAY 14, 1992**

A.3 CITY OF HOLLISTER STREETScape IMPROVEMENT PLANS

Standards for Streetscape

PART III

DESIGN STANDARDS

INTRODUCTION

This section of the guide presents required design standards which must be incorporated into streetscape designs in the City of Hollister. The following design standards have been divided into five categories, site preparation, hardscape elements, soundwalls, planting, and irrigation.

SITE PREPARATION

1. All site preparation work shall employ currently acceptable landscape industry methods.
2. All project areas shall be completely cleared, de-weeded, and sterilized prior to construction.
3. Soils shall be amended to improve infiltration, water retention, and structure. Although specifications for soil amendments shall be specific to the soil conditions on the project site, generally a two to three inch thick layer of amendment should be applied and incorporated into the soil. Amendment should usually consist of organic matter such as peat moss, compost, leafmold, or ground bark.
4. For slopes greater than 5%, appropriate erosion control methods shall be incorporated into the design. In addition to planting and/or applying seed, erosion control might include jute netting, soil stabilizers, terracing, and retaining walls.

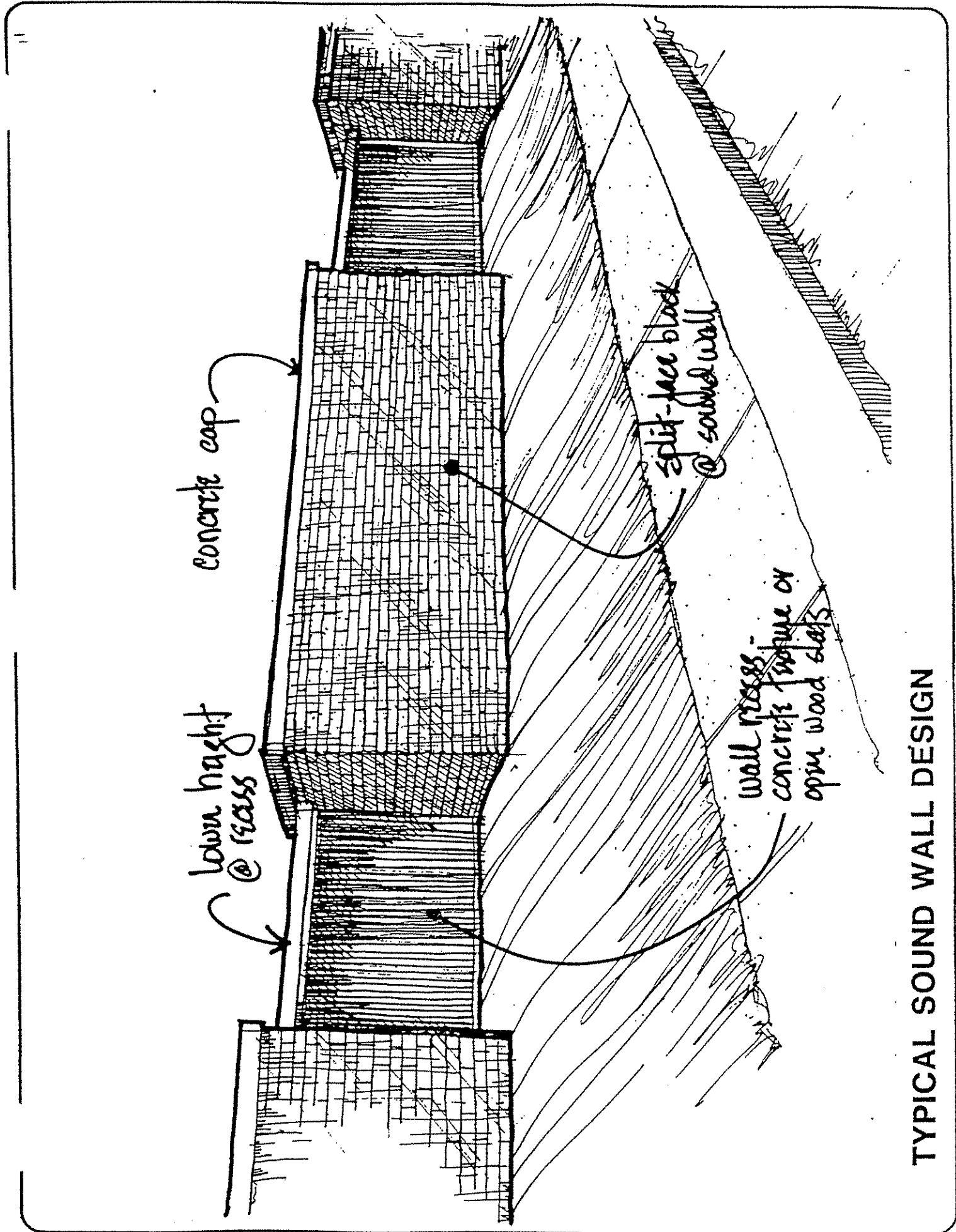
HARDSCAPE ELEMENTS

1. Hardscape elements might include such improvements as decorative pavement, walls, decorative fountains, and tree grates.
2. The use of hardscape elements can serve such purposes as enhancement of the project design, limiting ground level planting, and designating a project entrance.

3. The hardscape materials selected for streetscape projects shall be of high quality and installed in a professional manner. It is important that the selected materials be appropriate for the location and function and should relate to or repeat elements of the project design.

SOUNDWALL / ACOUSTICAL FENCES

1. The requirement for the installation of a soundwall or acoustical fence shall be determined by the City Planning and Engineering Departments. The City will also determine the location and height.
2. The soundwall or acoustical fence shall be designed by a civil or structural engineer licensed in California. The City may also require that an acoustical engineer determine the acoustical properties of the wall or fence.
3. Structural calculations shall be prepared in accordance with the Uniform Building Code edition currently adopted by the City of Hollister. The calculations shall verify that the stresses for concrete, reinforcing steel, welding and soil do not exceed allowables imposed by the code.
4. Walls and fences shall include articulations at regular approximate 80 foot intervals. When design constraints require a significant increase in the interval lengths a means of increased articulation emphasis must be included. In no case shall a wall or fence exceed 150 feet in length without articulation. Methods of articulation may include the following:
 - a. A minimum of a 24-inch change of plane
 - b. A minimum of a 18-inch change of height
 - c. A section of semi-open fence (if sound attenuation requirements allow)
 - d. A change of material and/or color
5. Walls and fences along a public street shall have a minimum six-foot wide landscaped setback from the back of sidewalk or back of curb (whichever is closer to the fence).
6. Walls and fences visible from public streets shall be constructed of durable, high-quality materials and shall incorporate a high quality in finish and detail. For sound attenuation purposes, walls shall be constructed of unit masonry and should be compatible with the project architecture. Walls of lesser quality of material and finish may be considered for approval if they are continuously screened with landscaping.



concrete cap

low height
@ 1500

split-face block
@ solid wall

wall recess -
concrete texture or
open wood slats

TYPICAL SOUND WALL DESIGN

PLANTING

1. Plant Selection

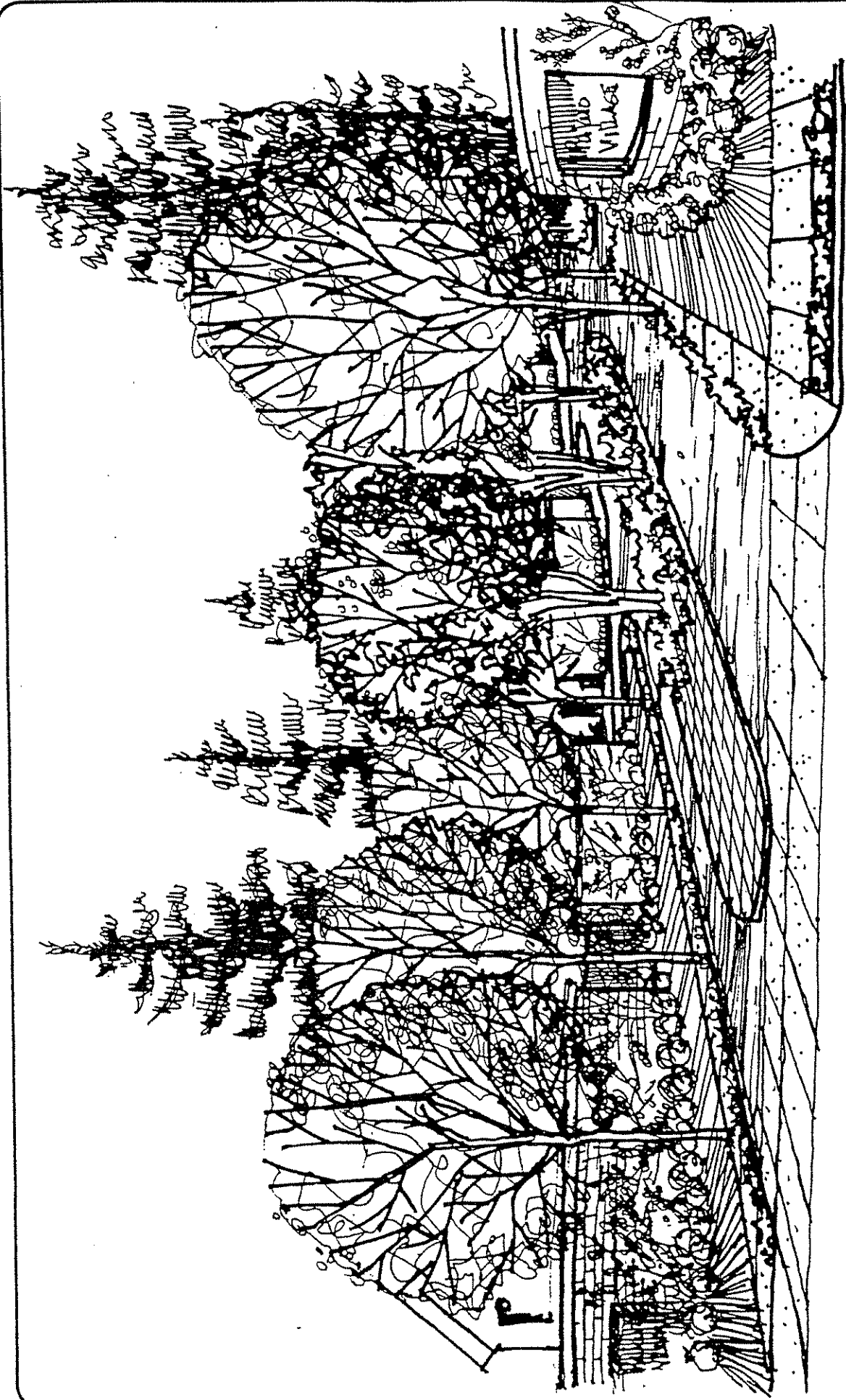
- a. The number of plant species shall be limited to create a consistent, unified design.
- b. Plant species with similar environmental requirements (i.e., water, sun exposure, soils) should be grouped together.
- c. The use of species with seasonal interest should be maximized.
- d. Plants shall be spaced and sized to create a mature appearance within three years.
- e. The ultimate size and character of the plant material must be considered in the design.
- f. Plants which drop excessive seed pods and fruit shall be avoided.

2. Street Trees

- a. Street trees are required at a maximum spacing of 30 feet along all public streets.
- b. Street trees may be located in tree wells within the sidewalk, within planting strips between the sidewalk and the curb, (minimum 3 feet wide), or within planting strips between the sidewalk and the right-of-way (minimum five feet wide).
- c. Street trees must be a minimum size of 15-gallon.
- d. Street trees should be selected from the Plant Selection List, (attached as Appendix E), unless a specific design reason exists which dictates an alternative selection.
- e. A tree grate shall be used if street trees are located within the sidewalk. The tree grate shall be similar in design to the Starburst Series manufactured by IronSmith, Inc.

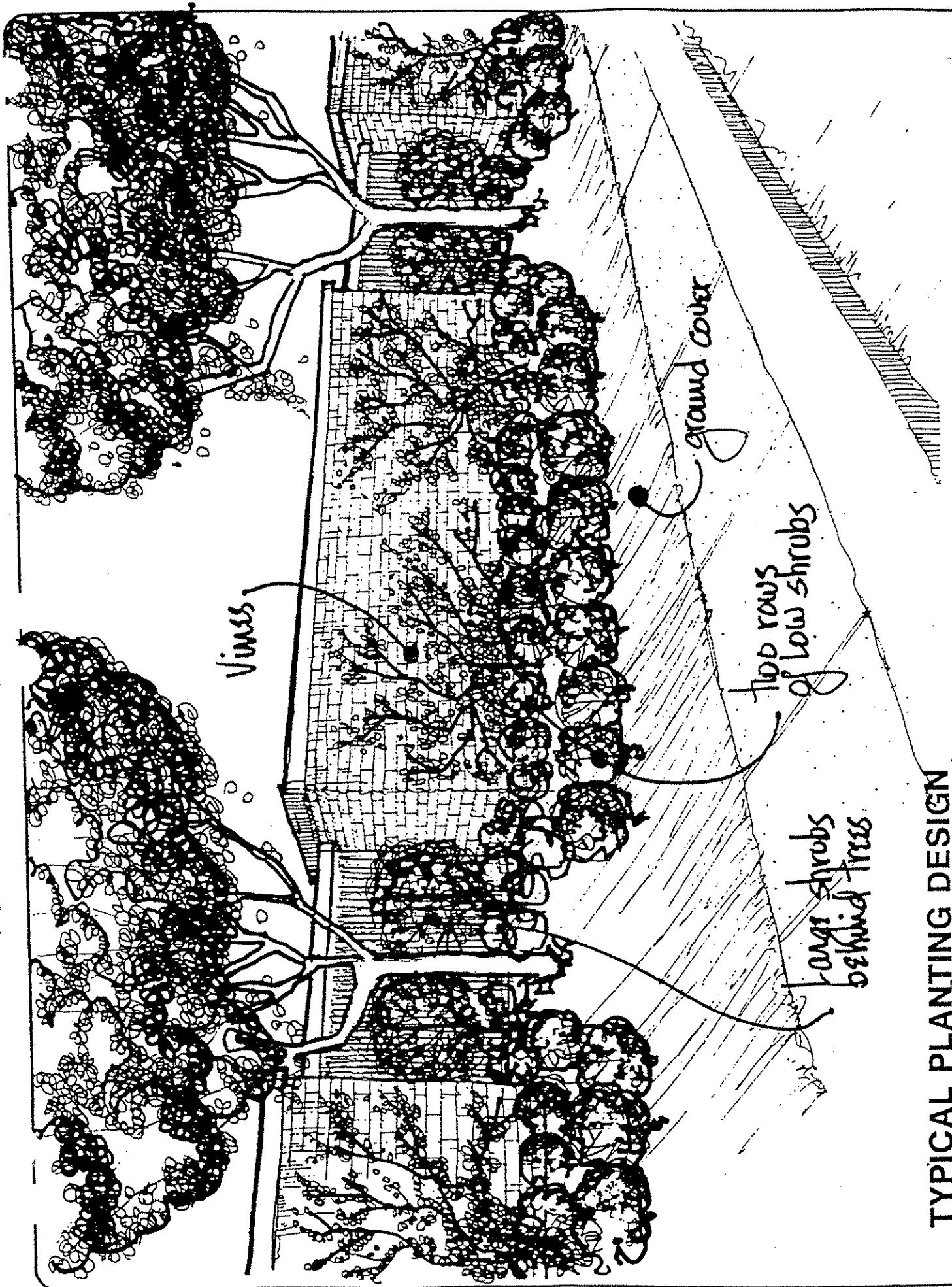
3. Medians and Project Entrances

- a. Planting and mounding heights must take into consideration the necessary sight distances from corners and median breaks. (The City Engineering Department should be consulted for project-specific restrictions.)
- b. Mounding up to 2:1 slopes may be used in medians with widths greater than eight feet and at project entrances.



TYPICAL PROJECT ENTRY DESIGN

- c. Weed growth shall be controlled with ground cover and/or heavy mulching.
 - d. Decorative paving shall be used in the "noses" of medians and in medians with widths less than three feet for maintenance purposes.
 - e. Shrub plantings, at maturity, shall not extend closer than 12 inches to the curb within medians.
 - f. Only 'clean' plant species shall be used in medians and at project entrances, (i.e., minimal fruit, seed pod, and leaf drop).
4. Planting Strips between Sidewalk and Soundwall
- a. Planting strips shall be a minimum of six feet wide.
 - b. Planting should incorporate a combination of trees, shrubs, and vines where appropriate.
 - c. Ground cover and/or heavy mulch shall be used under all shrubs and trees to prevent weed growth.
 - d. Trees shall be placed approximately 3 feet from the sidewalk or wall.



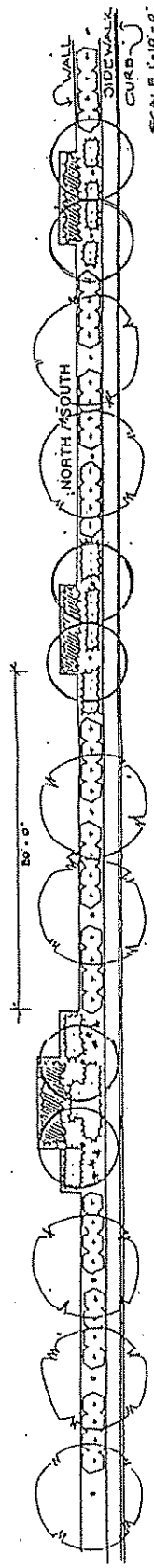
TYPICAL PLANTING DESIGN.

IRRIGATION SYSTEMS

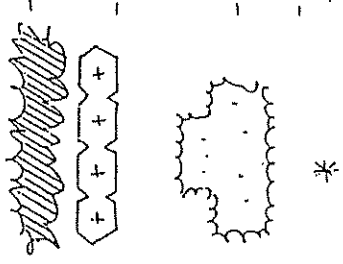
1. All irrigation designs (i.e. head layout and controller capabilities) shall apply adequate water without causing excessive runoff. The design shall be in conformance with City of Hollister Ordinance No. 752 which regulates water use. The use of bubbler systems, utilizing PVC lines and threaded type bubblers on PVC risers, shall generally be required.
2. Backflow prevention devices shall be as required by the City of Hollister (see typical detail, Appendix C), visually screened and protected with a heavy duty metal enclosure.
3. Gate valves shall be located to provide mainline isolation capabilities for ease of maintenance.
4. Sprinkler systems shall be separately circuited according to the following criteria:
 - a. Types of plant materials (ie, turf, ground cover, shrubs and trees) having significantly different water requirements.
 - b. Deep root systems and shallow root systems.
 - c. Different sun and wind exposures.
 - d. Soil differences (ie, sandy, gravelly, etc.).
 - e. Site grading (ie, high points and low points).
 - f. Precipitation rate of sprinklers (each circuit shall have matched precipitation rate).
5. Sprinkler piping shall have a maximum flow velocity of five feet per second.
6. Sprinklers with a 15' radius throw or less shall be spaced no further apart than $0.6 \times$ the manufacturer's listed diameter for the designed operating pressure. Sprinklers with a radius throw more than 15' shall be spaced no further apart than $0.5 \times$ the manufacturer's listed radius for the designed operating pressure. Special attention shall be given to the prevailing winds in the area. In windy areas, spray or rotor heads may need to be spaced closer than this minimum to counteract wind-caused distribution pattern distortion. Low angle trajectory nozzles shall be used where necessary to minimize wind drift onto adjacent pavements.
7. Sprinklers adjacent to walks, curbs, headerboards, etc. in shrub and groundcover areas shall be flush mounted using 6" or 12" pop-up sprinklers (appropriate with ground cover height).

8. All systems shall be designed for no greater than a 15% operating pressure differential from the first head to last head, and no more than the manufacturer's recommendations.
9. All sprinklers shall be adjusted to avoid spray onto walls, windows, roadways, signage and walks.
10. Sprinklers subject to low head drainage shall have internal or riser mounted check valves.
11. Provide pressure regulation where pressure exceeds recommended operating pressures for the type of sprinkler head used or when excessive misting occurs.
12. Where irrigation lines and wires cross roadways, walks or walls, sleeves and conduits shall be provided.
13. Drip Systems:
 - a. The use of drip/micro irrigation systems for special needs may be considered. Design to appropriately reflect permanency of system and incorporate features to insure reliability and maintainability of the system. (Drip systems to temporarily provide irrigation to establish plant material is an exception to this requirement.) Rodent and vandalism resistance, and accessibility for maintenance should all be considered.
 - b. Each emitter zone shall have its own strainer/filter with filtration to meet emitter requirements.
 - c. Each emitter lateral line and sub-lateral shall have an end flushing device.
 - d. The proper number of emitters or outlets shall be specified to deliver appropriate water for each type of plant.
14. The entire irrigation system is required to complete the cycle to accomplish ET requirements within an 8 hour period per night (six nights per week) under peak operating conditions. Drip or bubblers may operate outside the 8 hour schedule on a 16 hour basis.
15. Moisture sensing devices are encouraged for soil moisture control where practical. Consideration shall be given to the number and location of moisture sensors due to different soil conditions, elevations, exposures, planting, etc. Installation must be checked by the manufacturer's representative and certified as being correctly located and installed.

16. In lieu of using moisture sensors, irrigation schedules shall be submitted to reflect the amount of water needed to maintain adequate plant health based on actual water needs. Submit schedule for each month of watering season. Address water requirements during plant establishment period.
17. Fertilizer injection is encouraged. Consideration must be given to the housing of the equipment to prevent vandalism.
18. Looping of mainlines is encouraged when it will allow a reduction in mainline pipe size and reduce installation costs.
19. Recommended irrigation equipment is listed in Appendix F and should be used unless prior approval for substitutions is granted by the City of Hollister Engineering and Maintenance Departments.

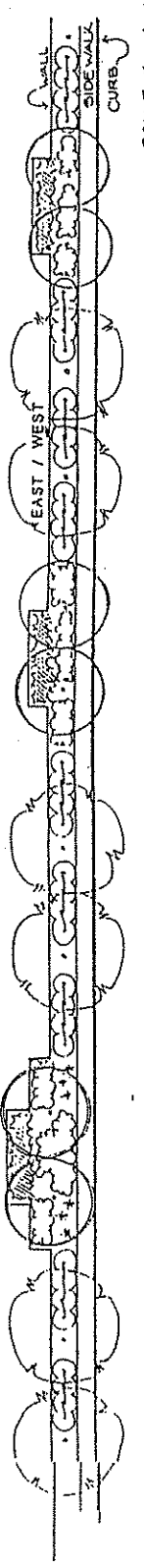


A PLAN - FOR NORTH / SOUTH STREET SCAPE

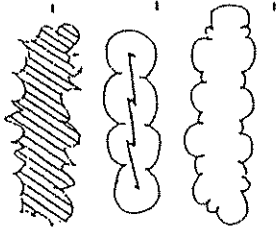


BOTANICAL NAME - FOR LARGE SHRUBS	COMMON NAME	SIZE
CEANOETHUS ARBOREUS 'RAY HARTNEY'	RAY HARTNEY CEANOETHUS	15' TALL
LEPTOSPERMUM SCOPARIUM 'KEATY'	AUSTRALIAN TEA TREE	10' x 10'
LEPTOSPERMUM SCOPARIUM 'KEATY'	WAX LEAF PRIVET	8' x 8'
BOTANICAL NAME - FOR MEDIUM SHRUBS	COMMON NAME	SIZE
ABELIA GRANDIFLORA*	GLOSSY ABELIA	8' x 5' spread
ECKLONIA FASTIGIOLATA*	PIRE OF MADAGASCAR	5' x 4'
ESCALONIA EXONIENSIS 'FREDER'	PINK ESCALLONIA	4'-6' x 4'-6' spread
NEROLIUM OLEANDER	PETITE SALADIN	5' x 4'
PITTOSPORIUM TOBIRA	HOCK ORANGE	5'-15' x 6'
BOTANICAL NAME - FOR SMALL SHRUBS	COMMON NAME	SIZE
AGAPANTHUS	A. AFRICANUS	2' x 1'
BOSSISUS TRICUSPIDATA	BOSTON IVY	4' x 4' spread
CITRUS POMELO	ORANGE HUCKLEBERRY	2' x 2'
DIETES BAKERI	DIETES BAKERI	2' x 2' spread
MYRTUS COMPAURIS 'COMPAUSTA'	DWARF MYRTLE	2'-5' x 4' spread

BOTANICAL NAME - FOR LARGE TREES	COMMON NAME	SIZE
QUERCUS ILEX	HOLLY OAK	40'-70' x 30'-40' spread
CERATONIA SIQUUA	CAIGIE TREE	30'-40' x 30'-40' spread
PYRUS KAWANOMII	BRADFORD PEAR	15'-25'
BOTANICAL NAME - FOR SMALL TREES	COMMON NAME	SIZE
PRUNUS BURBANKA THUNDERCLOUD	FLOWERING PLUM	25' x 30'
CALLISTEMON LANCEOLATUS	BOTTLE BRUSH	15' x 15'



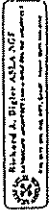
B PLAN - FOR EAST / WEST STREET SCAPE



BOTANICAL NAME - FOR LARGE SHRUBS	COMMON NAME	SIZE
PHOTINIA FRASERI	FRASER'S PHOTINIA	10' x 10'
PODOCARPUS GRACILIOR	FERN PINE	15'-40' x 10'-30'
VIBURNUM THURUS	LAURESTINUS	5'-10' x 4'-6'
XYLOSMA CONGESTUM	SHINY XYLOSMA	5'-10' x 10'
BOTANICAL NAME - FOR MEDIUM SHRUBS	COMMON NAME	SIZE
BERBERIS THUNBERGII	JAPANESE BARBERY	4'-5' x 4' spread
BOSSISUS TRICUSPIDATA	BOX LEAF HEBE	5' TALL
CITRUS POMELO	ORANGE HUCKLEBERRY	8' x 5'
JUNIPERUS SABINA 'TAMARISCIFOLIA'	TAMARISK WEEPERS	15'
PITTOSPORIUM VARIEGATUM	PITTOSPORIUM	5' x 6'
BOTANICAL NAME - FOR SMALL SHRUBS	COMMON NAME	SIZE
PITTOSPORIUM TOBIRA 'WHEELERII'	WHEELERS DWARF	2' x 2'
RAPHIDOLEPIS INDICA	R.I. BALLERINA	2' x 2'
COLECHENA PULCHRUM	PULCHRUM PINK DIOSMA	3' x 3'
VERBERNA	V. PERUVIANA	5' x 2'
CARYOPHYTEUM FRUTESCENS	C. HYSSOPIFOLIA	5' x 2'
	WHITE MARGUERITE	1 1/2' x 2 1/2'

BOTANICAL NAME - FOR LARGE TREES	COMMON NAME	SIZE
PLATANUS BICOCCOD	STAMBORE	40' x 60'
PINUS CAYENENSIS	CANARY ISLAND PINE	60' x 12'
BOTANICAL NAME - FOR SMALL TREES	COMMON NAME	SIZE
LAGERSTROEMIA INDICA	ORANGE MYRTLE	20' x 15'
GEUERA PARVIFLORA	AUSTRALIAN WILLOW	20' x 15'

CITY OF HOLLISTER
PLANTING CONCEPT
MASTER PLAN



SUGGESTED PLANT LIST FOR
NORTH AND SOUTH STREET SCAPE

<u>DESCRIPTION</u>	<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>SIZE</u>	<u>COMMENTS</u>
LARGE SHRUB	CEANOOTHUS ARBOREUS 'RAY HARTMEN'	RAY HARTMEN CEANOOTHUS	15' TALL	EVERGREEN
	LEPTOSPERNUM SCOPARIUM 'KEATEYII'	AUSTRALIAN TEA TREE	10'x 10'	EVERGREEN
	LIGUSTRUM JAPONICUM 'TEXANUM'	WAX LEAF PRIVET	8'x 8'	EVERGREEN
MEDIUM SHRUBS	ABELIA GRANDIFLORA	GLOSSY ABELIA	8'x 5' spread	EVERGREEN
	ECHIUM FASTUOSUM	PRIDE OF MADEIRA	5'x 4'	EVERGREEN
	ESCALLONIA EXONIENSIS 'FREDES'	PINK ESCALLONIA	4'- 6' x = spread	EVERGREEN
	NERIUM OLEANDER	PETITE SALMON	5'x 4'	EVERGREEN
	PITTIOSPORUM TOBIRA	MOCK ORANGE	6'- 15'x 6'	EVERGREEN
	SMALL SHRUBS	AGAPANTHUS	A. AFRICANUS	2'x 2'
PARTHENOCISSUS TRICUSPIDATA		BOSTON IVY	spreading	EVERGREEN
CISTUS PURPUREA		ORCHID ROCKROSE	4'x = spread	EVERGREEN
DIETES (MOREA)		DIETES BI-COLOR	2'x 2'	EVERGREEN
MYRTUS COMMUNIS 'COMPACTA'		DWARF MYRTLE	2'- 5'x = spread	EVERGREEN
LARGE TREES		QUERCUS ILEX	HOLLY OAK	40'- 70'x = spread
	CERATONIA SILIQUA	CAROB TREE	30'- 40'x = spread	EVERGREEN
	PYRUS KAWAKAMII	BRADFORD PEAR	15'- 25'	DECIDUOUS
	PRUNUS BLIREIANA THUNDERCLOUD	FLOWERING PLUM	25'x 20'	DECIDUOUS
SMALL TREES	CALLISTEMON LANCEOLATUS	BOTTLE BRUSH	15'x 15'	EVERGREEN

SUGGESTED PLANT LIST FOR
EAST AND WEST STREET SCAPE

<u>DESCRIPTION</u>	<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>SIZE</u>	<u>COMMENTS</u>
LARGE SHRUBS	PHOTINIA FRASERI	FRASER'S PHOTINIA	10'x 10'	EVERGREEN
	PODOCARPUS GRACILIOR	FERN PINE	15'- 40'x 10'- 30'	EVERGREEN
	VIBURNUM TINUS	LAURESTINUS	5'- 10'x 4'- 6'	EVERGREEN
	XYLOSMA CONGESTUM	SHINY XYLOSMA	8'- 10'x 10'	EVERGREEN
		BERBERIS THUNDBERGII	JAPANESE BARBERRY	4'- 6'x = spread
MEDIUM SHRUBS	HEBE BUXIFOLIA	BOX LEAF HEBE	5' TALL	EVERGREEN
	RAPHIOLEPIS INDICA	'SPRINGTIME'	8'x 5'	EVERGREEN
	JUNIPERUS SABINA 'TAMARISCIFOLIA'	TAMARAX JUNIPER	18"	EVERGREEN
	PITTIOSPORUM VARIGATED	PITTIOSPORUM	5'x 6'	EVERGREEN
		PITTIOSPORUM TOBIRA 'WHEELERII'	WHEELERS DWARF	2'x 2'
SMALL SHRUBS	RAPHIOLEPIS INDICA	R.I. BALLERINA	2'x 2'	EVERGREEN
	COLEONEMA PULCHRUM	PULCHRUM PINK DIOSMA	3'x 3'	EVERGREEN
	VERBINA	V. PERUVIANA	spreading	EVERGREEN
	CUPHEA	C. HYSSOPIFOLIA	6"- 2'	EVERGREEN
	CHRYSANTHEMUM FRUTESCENS	WHITE MARGUERITE	1 1/2" - 2 1/2"	EVERGREEN
LARGE TREES	PLATANUS BLOODGOOD	SYCAMORE	40'x 80'	DECIDUOUS
	PINUS CANARIENSIS	CANARY ISLAND PINE	60'x 12'	EVERGREEN
	LAGERSTROEMIA INDICA	GRAPE MYRTLE	20'x 15'	DECIDUOUS
SMALL TREES	GEIJERA PARVIFLORA	AUSTRALIAN WILLOW	20'x 15'	EVERGREEN